

# MEETING — AGENDA —

*Ngā Take*

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# OPERATIONS AND MONITORING COMMITTEE

*Komiti Whakahaere*

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**OP18**  
**Thursday, 11 April 2019**  
**Council Chambers**  
**Barkes Corner, Tauranga**  
**9.30am**

# Notice of Meeting No. OP18 Te Karere

## Operations and Monitoring Committee Komiti Whakahaere

**Thursday, 11 April 2019**  
**Council Chambers**  
**Barkes Corner**  
**9.30am**

His Worship the Mayor

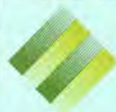
G J Webber

Councillors:

D Thwaites (Chairperson)  
J Palmer (Deputy Chairperson)  
G Dally  
M Dean  
M Lally  
P Mackay  
K Marsh  
D Marshall  
M Murray-Benge  
J Scrimgeour  
M Williams

Media  
Staff

Miriam Taris  
**Chief Executive Officer**  
**Western Bay of Plenty District Council**



*Western Bay of Plenty*  
*District Council*

Te Kaunihera a rohe mai i nga Kuri-a-Whare ki Otamarakau ki te Uru

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# **Operations and Monitoring Committee Delegations Mangai o Te Kaunihera**

## **Quorum:**

The quorum for this meeting is six members.

## **Role:**

Subject to compliance with Council strategies, policies, plans and legislation:

- To monitor performance and outcomes over all of Council's service functions including the following:
  - Regulatory Operations
  - Rooding Operations
  - Utilities Operations
  - Parks, Reserves, Cemeteries and other council property
  - Customer services, libraries
  - Corporate Property Operations and development
  - Sub Regional Parks
  - Operational service contracts (e.g. swimming pools, community halls)
  - Emergency Management
  - Rural Fire

## **General Delegations:**

- To make decisions to enable and enhance service delivery performance.
- To approve operational policy including resolving operational policy matters defined as the implementation of policy.
- To resolve all operational matters as referred by Community Boards.
- To make decisions in regard to assets to implement Council's plans, policies and projects as contained in the Long Term Plan and Annual Plans.
- To monitor assets and resources required for the delivery of services.
- To monitor the implementation of Council's strategies, plans, policies and projects contained in Council's Long Term Plan and Annual Plans.
- To receive and resolve on or recommend to Council or its Committees as appropriate the reports, recommendations and minutes of the Joint Road Safety Committee and any other Joint Committee, working group or forum as directed by Council.
- To undertake on behalf of Council all processes and actions (including consultation) for the amendment of bylaw schedules relating to operational services precedent to the recommendation to Council for adoption of the amendments.

**Financial Delegations:**

Pursuant to Section 4(1) of the Public Bodies Contracts Act 1959, the Committee shall have the power to enter into contracts in respect of the Committee's functions to a maximum value of \$5,000,000 for any one contract, provided that the exercise of this power shall be subject to, and within the allocation of funds set aside for that purpose in the Long Term Plan, the Annual Plan and Budget or as otherwise specifically approved by Council.

To report to Council financial outcomes and recommend any changes or variations to allocated budgets.

**Other:**

The Committee may without confirmation by Council exercise or perform any function, power or duty relating to those matters delegated by Council in like manner, and with the same effect, as the Council could itself have exercised or performed them.

The Committee may delegate any of its functions, duties or powers to a subcommittee subject to the restrictions on its delegations and provided that any sub-delegation to subcommittees includes a statement of purpose and specification of task.

The Committee may make recommendations to Council or its Committees on any matters to achieve the outcomes required in the role of the Committee but outside its delegated authorities.

# Agenda for Meeting No. OP18

Pages

**Present  
In Attendance  
Apologies**

## **Public Excluded Items**

The Council may by resolution require any item of business contained in the public excluded section of the agenda to be dealt with while the public are present.

## **Public Forum**

A period of up to 30 minutes is set aside for a public forum. Members of the public may attend to address the Committee for up to three minutes on items that fall within the delegations of the Committee provided the matters are not subject to legal proceedings, or to a process providing for the hearing of submissions. Speakers may be questioned through the Chairperson by members, but questions must be confined to obtaining information or clarification on matters raised by the speaker. The Chairperson has discretion in regard to time extensions.

Such presentations do not form part of the formal business of the meeting, a brief record will be kept of matters raised during any public forum section of the meeting with matters for action to be referred through the customer contact centre request system, while those requiring further investigation will be referred to the Chief Executive.

OP18.1

## **Presentation**

- Bay of Plenty Regional Council  
Water Quality Implications, Kaituna Diversion

OP18.2

## **Yeoman Walkway to Park Road Reserve Feasibility Study Report**

8-34

Attached is a report from the Reserves and Facilities Manager dated 19 March 2019.

OP18.3	<b>Additional Opening Hours at Te Puke Recycle Centre and Katikati Recycle Centre</b>	35-42
	Attached is a report from the Resource Recovery and Waste Team Leader dated 12 March 2019.	
OP18.4	<b>Omokoroa Golf Course and Precious Family Reserve Walkway Erosion Funding Report</b>	43-54
	Attached is a report from the Specialist Engineer dated 12 March 2019.	
OP18.5	<b>Housing Action Plan Six-Monthly Progress Report</b>	55-63
	Attached is a report from the Senior Policy Analyst dated 19 March 2019.	
OP18.6	<b>Infrastructure Services Report– April 2019</b>	64-76
	Attached is a report from the Deputy Chief Executive dated 26 March 2019.	
	The open section of the Operations and Monitoring Committee Information Pack No. OP18 has been circulated separately with the agenda.	

## **Local Government Official Information and Meetings Act**

### **Exclusion of the Public**

#### **Schedule 2A**

#### **Recommendation**

*THAT the public be excluded from the following part of this meeting namely:*

- *Waihi Beach Wilson Road Bore Site WSZ3 – Land Purchase – In Confidence*
- *Te Puke War Memorial Hall – Request for Funding - In Confidence*
- *Infrastructure Services Report – April 2019 - In Confidence*

The general subject to each matter to be considered while the public is excluded, the reason for passing this resolution in relation to each matter, and the specific grounds under Section 48(1) of the Local Government Official Information and Meetings Act 1987 for the passing of this resolution are as follows:

<b>General subject of each matter to be considered</b>	<b>Reason for passing this resolution in relation to each matter</b>	<b>Ground(s) under Section 48(1) for the passing of this resolution</b>
<i>Waihi Beach, Wilson Road Bore Site WSZ3 – Land Purchase</i>	<i>THAT the public conduct of the relevant part of the proceedings of the meeting would likely result in the disclosure of information for which good reason for withholding would exist.</i>	<i>To enable the Council to carry on without prejudice or disadvantage, negotiations (including commercial and industrial negotiations) Protect the privacy of natural persons, including that of deceased natural persons.</i>
<i>Te Puke War Memorial Hall – Request for Funding</i>	<i>THAT the public conduct of the relevant part of the proceedings of the meeting would likely result in the disclosure of information for which good reason for withholding would exist.</i>	<i>To enable the Council to carry on without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).</i>
<i>Infrastructure Services Report – April 2019 – In Confidence</i> <i>- Property Negotiations</i> <i>- Contract and Project Updates</i> <i>- Waihi Beach Coastal Protection</i>	<i>THAT the public conduct of the relevant part of the proceedings of the meeting would likely result in the disclosure of information for which good reason for withholding would exist.</i>	<i>To enable the Council to carry on without prejudice or disadvantage, negotiations (including commercial and industrial negotiations). Protect the privacy of natural persons, including that of deceased natural persons.</i>

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**Western Bay of Plenty District Council**  
**Operations & Monitoring Committee**  
**Yeoman Walkway to Park Road Reserve**  
**Feasibility Report**

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**Purpose**

To consider the Yeoman Walkway to Park Road Reserve Feasibility Report and provide direction to staff on the next steps in the process.

The recommendation is that:

- \$50,000 be budgeted in 2019/20 to provide a retaining wall for safety.
- That the construction of the path be considered in future annual plans.

**Recommendation**

1. ***THAT the Reserves & Facilities Manager's report dated 19 March 2019 and titled Yeoman Walkway to Park Road Reserve – Feasibility Report be received.***
2. ***THAT the report relates to an issue that is considered to be of low significance in terms of Council's Significance and Engagement Policy.***
3. ***THAT staff prepare a internal submission to the 2019/20 Annual Plan and Budget for the following:***
  - ***\$50,000 for upgrade of the hazardous section of the existing pathway***
4. ***THAT the committee notes that the proposed walkway/cycleway is identified in the following planning documents:***
  - ***Walking and Cycling Strategy 2009***
  - ***Katikati/Waihi Beach Reserve Management Plan***

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Peter Watson  
**Reserves & Facilities Manager**

Approved




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Gary Allis  
**Deputy Chief Executive**



## 1. Background

During the publically consulted Katikati/Waihi Beach Ward Reserve Management Plan review, undertaken in 2018, Council identified a number of walkway/cycleway initiatives within the Reserve Management Plan, which are linked to Council's Walking and Cycling Strategy.

Copied below are the relevant objectives/policies from the Katikati/Waihi Beach Ward Reserve Management plan adopted in December 2018 that relate to the proposed cycleway/walkway along the esplanade reserve between The Yeoman Walkway and Park Road Reserve, Katikati.

***Reserve Management Policy:***

*6.82.1 Continue to secure remaining areas of esplanade reserve around the Katikati Peninsula (as identified in the District Plan) to achieve continuous pedestrian access and harbour protection.*

*6.82.2 Where existing access over private land facilitates the coastal walkway/cycleway connection, seek to secure some form of legal formalisation of this access including access to Tutaetaka.*

*6.82.3 Manage the coastal esplanade for the protection of the natural character and wildlife values of the Tauranga Harbour.*

*6.82.4 Consistent with the protection of the above values provide for continuous pedestrian/cycleway linkage around the Katikati Peninsula.*

***6.82.5 Develop a walkway/cycleway between the Yeoman Walkway, Riverlea Drive Reserve, Levley Lane Reserve, Francis Drive Reserve and Park Road Reserve next to the retirement village.***

Source: Page 248 of the Katikati/Waihi Beach Reserve Management Plan – 13 December 2018

In December 2018, Council commissioned a scoping report on the feasibility of creating or forming the section of pathway identified in section 6.82.5 (above) of the Katikati/Waihi Beach Reserve Management Plan.

The Feasibility Report has been completed.

**Attachment A**

The Feasibility Report identifies the work required to form a walkway/cycleway, including: environmental considerations; consenting requirements; assessment of construction material and methodology; and an estimate of costs for the project including the options of concrete or gravel surfacing.

The report identifies the practical aspects and options associated with upgrading and forming an all-weather multi-use pathway, and goes into some detail as this was required to ensure firstly, that the route was feasible to establish an all weather pathway and secondly to have a clear understanding of cost estimates. The report is separated into four sections.

Section 1 - Photo essay of suggested route  
Section 2 - Route and bench-line construction detail  
Section 3 - Scope of work for earthworks  
Section 4 - Rough order of cost estimate

Appendices I – Resource consent considerations  
II – Pavement options and rationale

Note: Appendix III – Schedule of rates and detailed estimates, has been redacted from this report as the rates are commercially sensitive and could be used by any tenderers, should Council decide to proceed with the proposal.

Figure 1: Indicative route along esplanade reserve(s) – Not to scale



## 2. Significance and Engagement

In terms of the Significance and Engagement Policy this decision is considered to be of low significance because the financial implications will be considered through the 2019/20 Annual Plan and Budget process.

### 3. Engagement, Consultation and Communication

Interested/Affected Parties	Completed/Planned Engagement/Consultation/Communication
Name of interested parties/groups	<ul style="list-style-type: none"> <li>• Council has worked with the Katikati Trails Development Group for that past 3-4 years.</li> <li>• The Operations and Monitoring Committee at it's meeting on 28 February 2019 considered a petition from adjoining residents.</li> </ul>
Tangata Whenua	<ul style="list-style-type: none"> <li>• Consultation was done through the Katikati/Waihi Beach Reserve Management Plan review process.</li> <li>• An archaeological assessment for the proposal will involve consultation with tangata whenua</li> </ul>
General Public	Consultation was undertaken on Council's Walking and Cycling Strategy in 2009 and more recently in 2018, consultation was undertaken during the review of the Katikati/Waihi Beach Reserve Management Plan.

### 4. Issues and Options Assessment

Option A	
<p><b>3. That staff prepare a internal submission to the 2019/20 Annual Plan and Budget for the following;</b></p> <ul style="list-style-type: none"> <li>• <b>\$50,000 for upgrade of the hazardous section of the existing pathway</b></li> </ul>	
<p><b>Assessment of option for advantages and disadvantages taking a sustainable approach</b></p>	<ul style="list-style-type: none"> <li>• An upgrade to an existing hazardous section of the pathway will be completed thereby reducing health and safety risks to users of the pathway.</li> <li>• The pathway aligns with the Reserve Management Policy in the Katikati/Waihi Beach Reserve Management Plan</li> <li>• The pathway aligns with the Walking and Cycleway Strategy</li> <li>• The pathway aligns with the Recreation and Leisure Strategy outcomes and objectives</li> </ul>
<p><b>Costs (including present and future costs, direct, indirect and contingent costs) and cost effectiveness for households and businesses</b></p>	<ul style="list-style-type: none"> <li>• Capital costs will need to be considered through future 2019/20 Annual Plan and Budget process.</li> <li>• If pavement construction is done with concrete there will be overall less operational lifecycle costs, along with a better level of service provided to the various users of the pathway.</li> </ul>

<b>Option B</b>	
<p><b>3. That does not staff prepare a internal submission to the 2019/20 Annual Plan and Budget for the following;</b></p> <ul style="list-style-type: none"> <li><b>\$50,000 for upgrade of the hazardous section of the existing pathway</b></li> </ul>	
<p><b>Assessment of option for advantages and disadvantages taking a sustainable approach</b></p>	<ul style="list-style-type: none"> <li>The Reserve Management Policy in the Katikati/Waihi Beach Reserve Management Plan would not be achieved.</li> <li>The Walking and Cycleway Strategy outcome would not be achieved</li> <li>Recreation and Leisure Strategy outcomes and objectives would not be achieved.</li> </ul>
<p><b>Costs (including present and future costs, direct, indirect and contingent costs) and cost effectiveness for households and businesses</b></p>	<p>It is recommended that \$50,000 be set aside for the hazardous section of the existing pathway go ahead regardless of whether or not the walkway/cycleway proceeds.</p>
<b>Option C Status Quo</b>	
<p><b>Assessment of option for advantages and disadvantages taking a sustainable approach</b></p>	<ul style="list-style-type: none"> <li>The Reserve Management Policy in the Katikati/Waihi Beach Reserve Management Plan would not be achieved.</li> <li>The Walking and Cycleway Strategy outcome would not be achieved</li> <li>Recreation and Leisure Strategy outcomes and objectives would not be achieved.</li> </ul>
<p><b>Costs (including present and future costs, direct, indirect and contingent costs) and cost effectiveness for households and businesses</b></p>	<p>It is recommended that \$50,000 be set aside for the hazardous section of the existing pathway go ahead regardless of whether or not the walkway/cycleway proceeds.</p>

## 5. Statutory Compliance

The proposed walkway/cycleway aligns with the principles of the Reserves Act 1977 and is consistent with the specific reserve management policy in the Katikati/Waihi Beach Reserve Management Plan.

Statutory matters are considered in the attached Feasibility Report.

The provision and enhancement of access to and along waterways via esplanade reserves is consistent with the provision of the Resource Management Act 1991.

## 6. Funding/Budget Implications

Budget Funding Information	Relevant Detail
Council does not have any specific funding set aside for the construction phase of the proposed walkway/cycleway.	<p>It is recommended that staff prepare an internal submission to the 2019/20 Annual Plan and Budget process.</p> <p>Council has \$75,000 set aside in the 2018/19 Financial Year for the replacement of the culvert which is causing erosion along part of the Yeoman Walkway and this matches the \$88,000 identified and included in the feasibility report and budget.</p>

Option	Cost Estimate	Additional Funding Required
1. Status quo – but repair hazardous section and construct retaining walls	\$50,000	\$50,000
2. Compacted aggregate – recycled concrete or metal including retaining wall and bridge	\$340,000	Net cost to construct aggregate path \$200,000
3. Concrete pavement including retaining wall and bridge	\$470,000	Net cost to construct concrete \$330,000

### Notes:

- Options 2 and 3 include the \$50,000 required for the upgrade of the hazardous area and the \$88,000 for the remediation of the scour zone and culvert replacement.
- Fees relate to resource consent for culvert replacement and scour protection, Archaeological Authority, Cultural monitoring and Project Management costs.



## Investigation Report for Yeoman Walk/Cycle Pavement Construction from Park Road to Uretara Stop-bank (Riverlea Dr. west) at Katikati

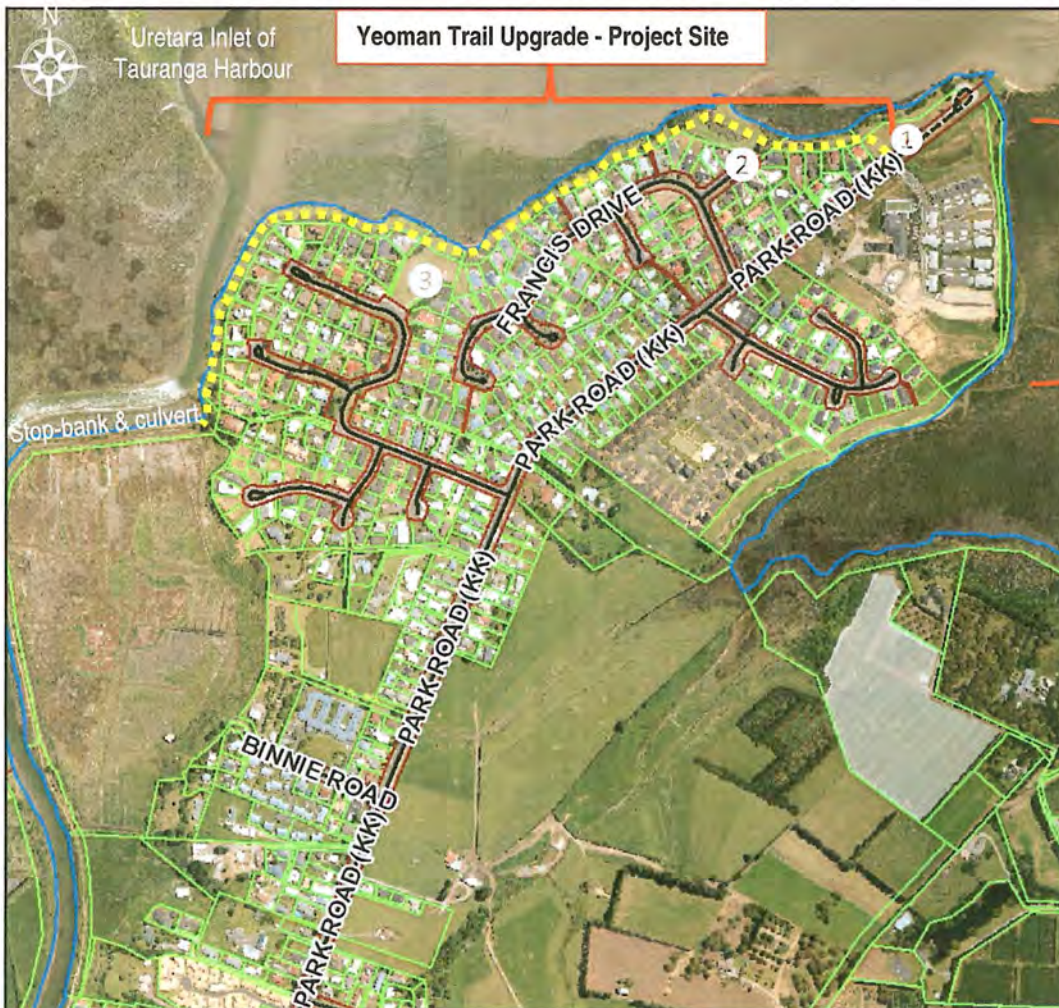


### Executive Summary

- This report documents the practical aspects and options associated with upgrading and forming an all-weather 1,305-metre-long path for combined pedestrian and cycle use on esplanade reserves and recreation reserves at Katikati. This proposed activity is consistent with Council’s Walking & Cycling Strategy which was adopted in 2009, and with the adopted 2018 Katikati/Waihi Beach Ward Reserve Management Plan review.
- The pathway is to generally follow the existing grassed walkway route from Park Road Reserve in the NE (meeting with Park Rd. opposite the entrance to Summerset Retirement Village) and to link with the eastern end of the stop-bank, being west of Riverlea Drive.
- It is anticipated that any decision to progress the trail construction will require approximately four-month construction period, which would include site clean-up.
- Resource consent assessment is addressed at Appendix I to this report – it concludes that subject to confirmation from planning staff that pavement construction will not adversely affect overland flow from neighbouring properties, then resource consents are not required.
- Construction options and a rough order of costs are covered at Appendices II & III respectively. Two items of work are high priority for attention, costing in the order of \$149,000. Depending on the methodology chosen, the total project cost is in the order of \$470,000.

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### Katikati Locality Map 1



**Northern Section of the Yeoman Trail**  
The yellow-dotted line indicates the general location of the 1,305m long x 2.5m wide paved walk/cycleway being assessed for construction over Council reserve land. The project site commences immediately north of Council Utilities infrastructure located approx. 50m from the end of Park Road. It extends generally westward to the culvert that crosses beneath the Uretara Stream stop-bank.

- Construction access is available from;
- ① Park Road Reserve;
  - ② Irwin Court cul-de-sac (Francis Drive Reserve);
  - ③ Levley Lane Reserve



SECTION 1.

Photo Essay

The direction of travel in this photo essay is from Park Road Reserve travelling south-westward. This essay is to be interpreted in conjunction with information contained in Sec.2 of this report.



Park Rd. Reserve  
(looking east)



Park Rd. Reserve  
(looking south)



Photos 1 & 2

These two photos are of the same site, being the SW end of Park Road Reserve.

The photos show the recommended alignment in respect to:

- the existing pavement on the eastern side of Park Rd.;
- it passes between the reserve name sign & a double cesspit alongside the kerb-&-channel,
- it skirts around Utilities infrastructure (*buried sewerage pipes & pumps with above-ground electrics, breathers & service-lids*), thereby allowing sufficient utility service access and aesthetic separation;
- it minimises physical intrusion into the open recreational & aesthetic space of the reserve;
- it keeps a modest, but acceptable, separation distance from the neighbour's property;
- it takes an alignment which minimises the gradient of the pavement and requires the minimum of earthworks.

Photo 3 – Cross-sections 'A' & 'B' (p.8)

[Looking SW from SW corner of Park Rd. Reserve]

- The dashed-line route, illustrated here and in Photo 4, is closer to the harbour, offering water-views; this is the recommended option which passes on the seaward side of the middle-distant pohutukawa.
- The dotted line is the alternative route which is closer to the neighbouring properties. This route gives a better approach angle around the landward side of the middle-distant pohutukawa but it is likely to be a cause for neighbour angst.
- By skirting around the middle of the grassed area, both routes minimise disruption of the open green space.



**Photo 4 – Cross-sections 'C' (p.8)**  
 [The convergence of the two route options shown in Photo 3.]

The preferred route passes seaward (north) of the pohutukawa with 2m separation from the tree's base. To ease the trail gradient between the terrace and the slope toe, a 2m wide x 15m long cutting (as illustrated – brown) can be formed upon which the pavement may be laid. The 2m clearance, and construction care, is required to minimise pohutukawa root damage. The only alternatives to this methodology are either:

- A 12m long cantilevered timber boardwalk structure overhanging the bank with outside safety railing, or;
- Reposition the route landward of the tree which will exacerbate neighbour angst\*.
- Neither of these two alternatives would ease the trail gradient between the terrace and the slope-toe.

[\* The neighbour at 180 Park Rd. verbally offered to cost-share a solution which placed the trail on the seaward side of the tree.]



**Photos 5 & 6**  
 At Cross-sections 'D' & 'E' (p.8)

As stated at Photo 4, a 15m long cutting (from 0.0m to 0.6m deep), could extend around the seaward side of the pohutukawa (upper-middle of image). Over this distance it will reduce the trail width to approx. 2.0m (rather than 2.5m width as proposed for the rest of the route). This portion of reduced width is to avoid building the trail on fill and also to minimise root damage to the pohutukawa.

The balance of this section of trail would be 2.5m wide with a 1.2m to 2.0m high certified retaining wall on the upslope face and a retaining wall downslope being up to 1.0m high.

Approx. total retaining wall requirements along this portion of the route are:

- Upslope side = 60m long x 2.0m high.
- Downslope side = 50m long x 1.0m high (includes site beside redwoods at Photos 7–8).

Terrain above the 2.0m retainer needs to be shaped to 2h:1v batter then overlaid with biodegradable, coconut weed-mat and replanted with low-growing natives (*Coprosma repens* v. Poor Knights & *Coprosma kirkii*).

**Consent Considerations:**

The existing slope is an exposed or weed-covered batter approx. 4.5m high. The intention to infill variously 0.7m to 1.0m of the toe, then construct an upslope retaining wall up to 2.0m high, does not trigger the requirement for resource consent because the activity site is not within an S8 Landscape feature. Also, the earthworks are a Permitted Activity within ecological feature [T13/3 D03; Park Road Shoreline Estuarine Margin Vegetation] per Rule 5.4.1(d) of the District Plan.

**Building Code:**

A C.P. engineer's certification of the landward retaining wall will be required due to it being higher than 1.5m and the embankment surcharge on the wall from the batter above.

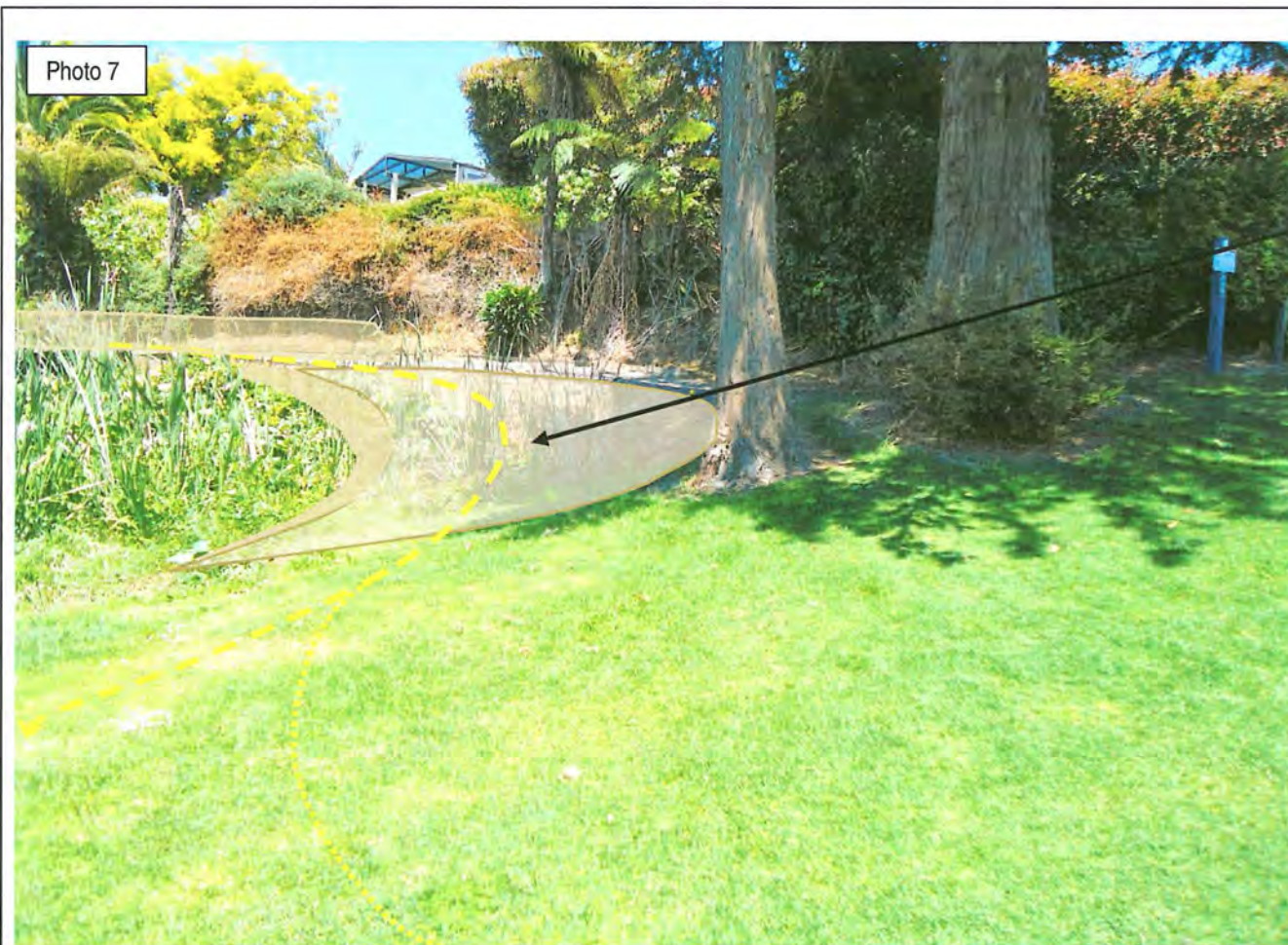
**Footnote:**

Existing vegetation on the upslope side of this trail portion comprises exclusively exotic species, some have been planted, but most are garden escapees and rank-growth weeds (some of which show in the photos as being sprayed by Council's contractor).



**Photo 6**





**Photos 7 & 8**  
**At cross-sections 'G' & 'H' – Francis Dr. Reserve**

- 50m<sup>2</sup> x 1.2m deep = 60m<sup>3</sup> of fill is required to infill the wet seep at the corner beside the two redwoods so that the trail can pass around the trees rather than destroy the smaller of the two trees. This fill will cover a patch of bulrush (15%) & weeds (85%)– arum lily, elephant ear, yellow-flag iris & convolvulus.  
*[Note 1. this infill represents less than 1% of the adjacent freshwater wetland within this portion of esplanade reserve.]*  
*[Note 2. the redwoods will gradually increase their girth to obstruct the existing path if the path isn't re-routed or a tree removed.]*
- The 'dashed-line' route, illustrated in these two photos is the recommended option.  
 This route:
  - avoids interruption to the cross-slope profile (*foreground of photo 8*), so allowing continued vehicular access to the passive beach-side picnic area and for beach launching/retrieval of small watercraft (*site circled blue*);
  - utilises the existing boardwalk within Francis Drive Reserve;
  - requires no cross-fall benching;
  - minimises the trail gradient between cross-sections 'J' & 'K' (*see p.8*).
- On sloping ground (beyond infill area), topsoil must be stripped to the minimum required width & depth to lay pavement flush to surroundings. On low-lying ground alongside the wetland, so as to ensure the pavement remains above wet winter ground conditions, the pavement will need to be boxed & laid over the existing ground profile after spraying out grass; however, the finished pavement profile needs to ensure approx. 2° cross-fall. Then the edges shall be dressed with topsoil & grass-seeded to marry into existing terrain.
- The 'dotted line' is the alternative route which passes on the landward side of a small freshwater wetland and is closer to the neighbouring properties (*see photo 10*). It:
  - does not give an ideal crossing for vehicles for boat launching/retrieval;
  - results in a short steep grade at the SW end of Francis Drive Reserve;
  - requires a moderate amount of cross-fall benching with associated greater construction cost.



Photo 9 (preferred route – foreground)  
At cross-section 'J' (p.8)

- Foreground – The route is on an easy grade, requiring minimal ground profile adjustment with site character and treatment being the same as detailed at points 2 & 3 of photo 8 above.
- Middle-distance – The trail route passes between two clusters of large pohutukawa trees on an easy grade, where the pavement would need to be laid on the existing ground and edge-dressed after.
- Background – The trail route rises to the terrace on a moderate grade. The pavement would need to be set into the ground. A minor cross-fall cut over 15m will be required, ensuring 2° cross-fall to sheet surface-water to the sides.

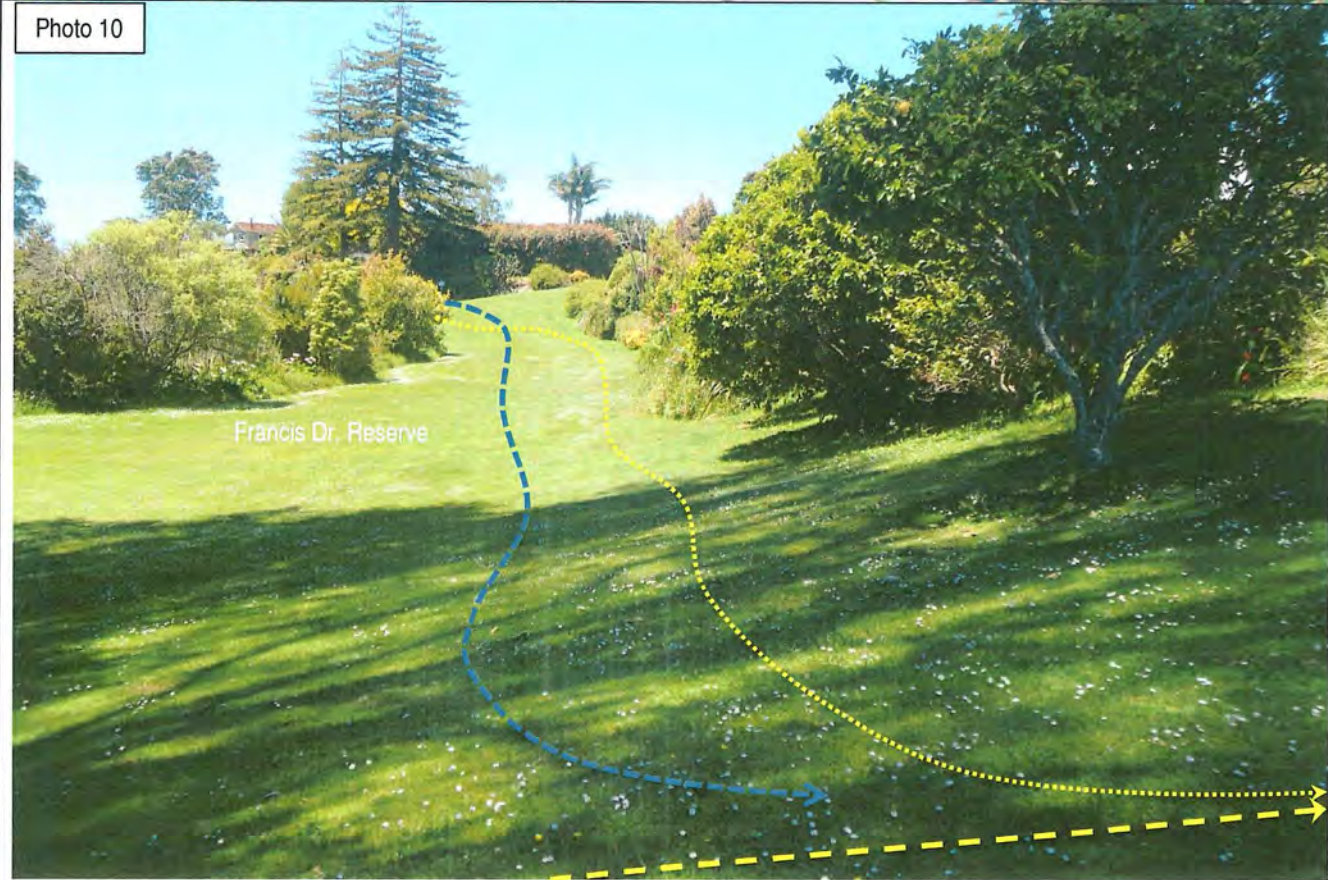


Photo 10 (alternate route)  
Cross-sections 'I', 'J', 'K' (p.8)

Most pedestrians use the route illustrated blue (through the swale rather than on the cross-slope) because it is a little shorter than the alternative (across the boardwalk). However, it can be wet in winter forcing people to walk on the cross-slope or use the boardwalk route.

This route would require the existing cross-slopes to be benched with cut-to-fill and importation of extra fill. Also, the foreground terrain requires a moderately steep climb to the terrace. This steep grade can only be eased by substantial infill & contouring of the cross-slope approach, or by using the alternative (preferred) route over the existing boardwalk.

Cost comparison of route options:

- (i) Preferred route – Photo 9, (dashed line):  
Extra 20 linear metres of concrete @ 2.5m wide equates to  $\$55/m^2 \times 2.5m \times 12m =$  say \$1,700.00
  - (ii) Alternate route – Photo 10, (dotted line shown here along the cross-slope):  
170m<sup>3</sup> extra fill supplied and laid @  $\$40/m^3 =$  Approx. \$6,800.00
- The preferred route is approx. \$5,000 cheaper.

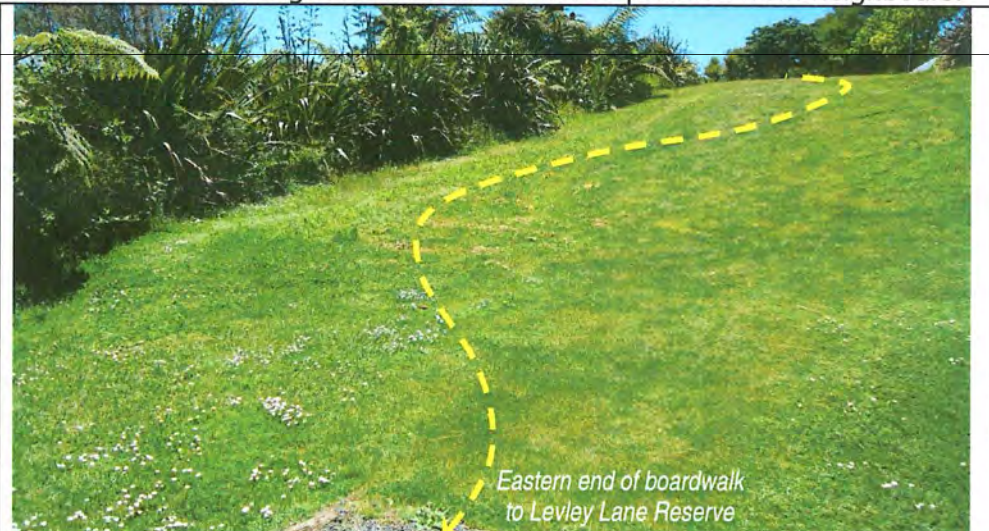
Photos 11 to 18 inclusive, below – [refer to the route map & cross-sections on p.8, Sheet 2]



Photos 11 & 12 (above): On the terrace passing 13-19 Francis Dr.; straight-forward pavement formation & alignment maximises neighbour separation.



Photos 13 & 14 (above): Passing 19–27 Francis Dr. There are no notable construction issues & alignment maximises trail separation from neighbours.



Eastern end of boardwalk to Levley Lane Reserve

Photos 15 & 16: Passing 31-43 Francis Dr. The route curvature optimises neighbour separation & minimises the gradient of the slope to the boardwalk.



Photos 16 & 17: From Levley Ln. Res. to 44 Levley Ln. The trail passes from Levley Ln. Res. through the gap in the trees seen above. West of Levley Lane Reserve the pathway can be closer to neighbours due to elevation separation; the indicated route largely follows the pattern of existing use.



Photo 18: Passing 44-56 Levley Ln.; the profile has 0.2m of cross-fall. [The red circle denotes the site referred to in photos 24-26 below –(see p.7)]

Photo 19: Passing 70-53 Levley Ln. [There are no photos between 56 & 70 Levley Lane.] Aside from comments below (at Sec.2.1, Sheets 2 & 3) there are no other construction issues through this portion of the trail.



Photo 20: Passes 13 Riverlea Dr. to 31C Levley Ln. Minor crown-lift of two trees is needed, as shown in this image, otherwise there are no issues.

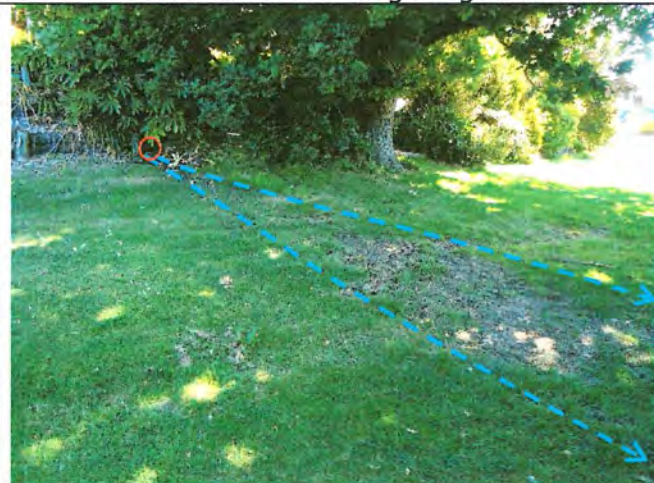
Photo 21: Passes alongside eroding embankment caused by culvert from the wetland inside the stopbank. The erosion, and culvert replacement by means of a flat-rack bridge & rock armouring, is currently being addressed with BOPRC and WBOPDC, via resource consent procedures.



Photo 22: Yeoman walkway stopbank – existing metal surfaced trail

Photo 23: Shows an example of a 2.5m wide concrete path; this photo taken at Huharua Park, Plummers Pt.

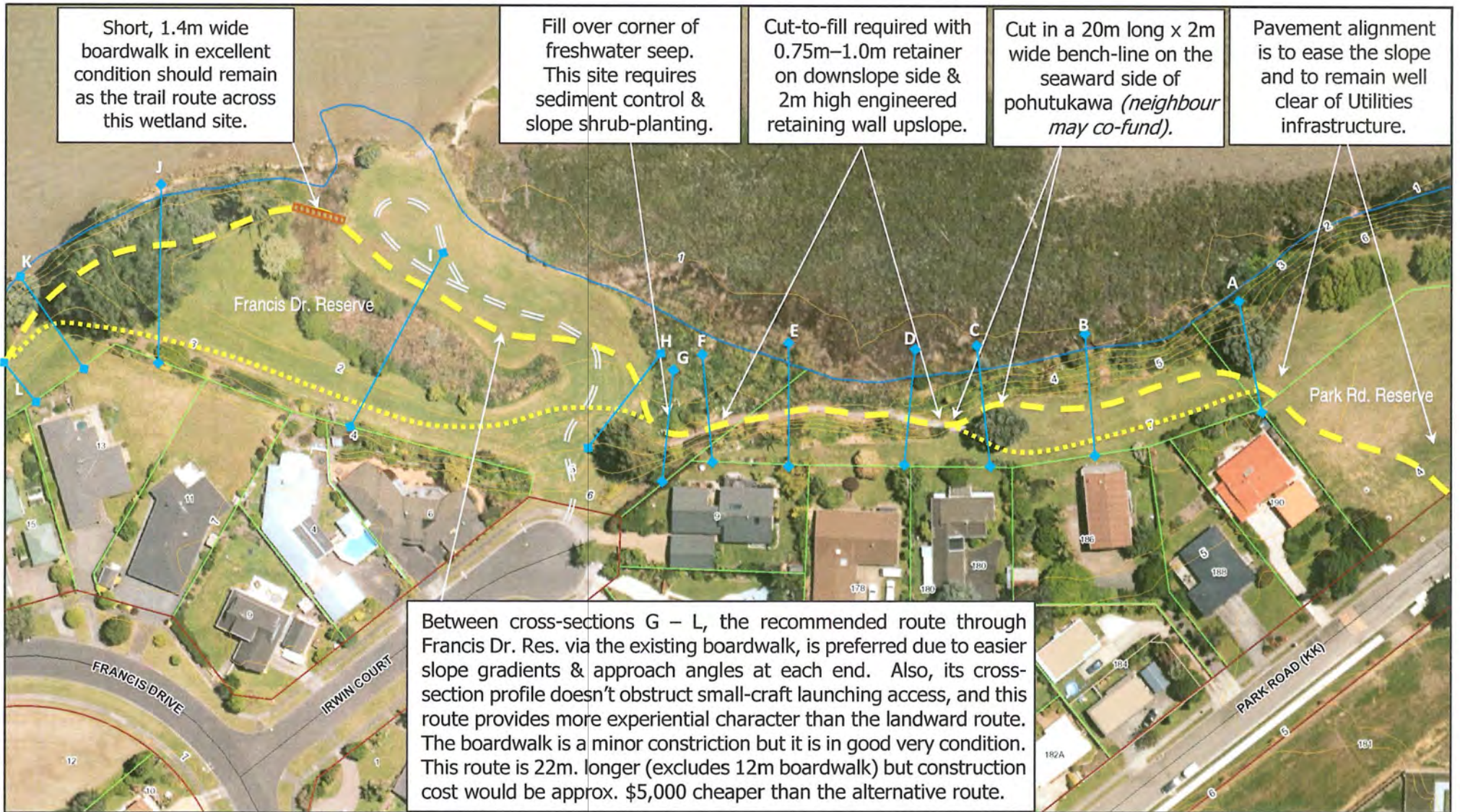
Photos 24 to 26: Stormwater outlet from neighbour at 56 Levley Lane discharges onto ground at landward edge of esplanade reserve with resultant outwash scour across walkway. This outlet needs to be extended to harbour-edge vegetation via a buried pipe at the neighbour's cost.



SECTION 2

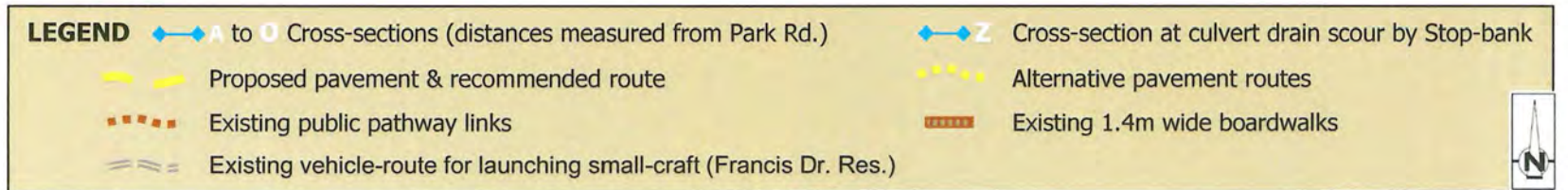
SECTION 2.1 **1,305m Long Pathway – Route and Bench-Line Construction Detail**

[Includes earthworks profiles & estimated volumes for esplanade walk/cycle path.]



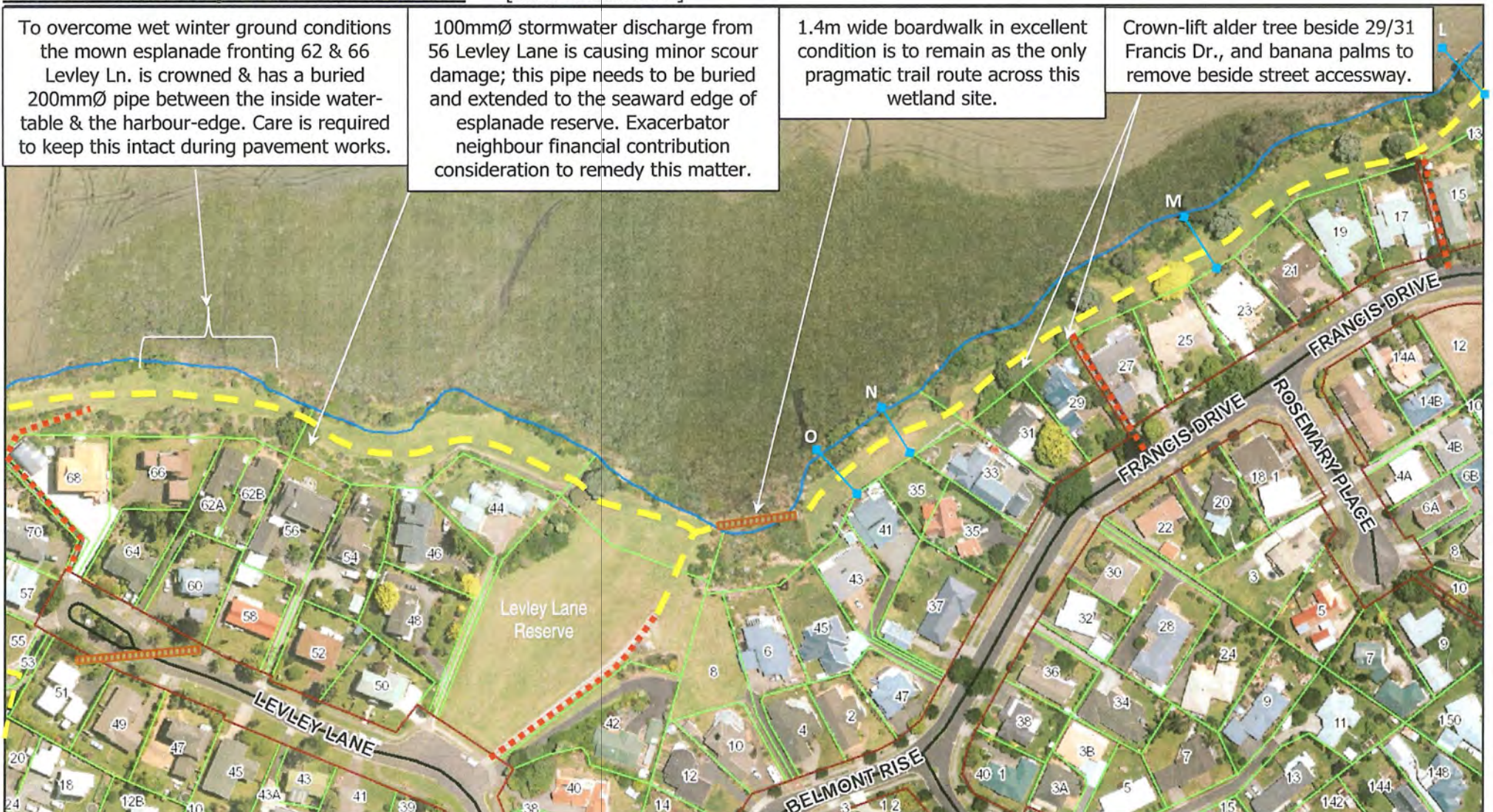
Sheet 1 – Route Map & Cross-section Sites (above)

[Scale 1:1800 at A3]



Sheet 2 – Route Map & Cross-section Sites

[Scale 1:2000 at A3]



Sheet 3 – Route Map & Cross-section Sites [Scale 1:2000 at A3]



In a general sense, the parameters of this investigation report extend to chainage 1,260m. Remediation of the culvert at chainage 1,305m, and the esplanade reserve edge repair between these two points, is being addressed via a separate resource consent process because, left unchecked the continuing scour erosion caused by the culvert and drain threatens the integrity of the Yeoman Walkway and the public access link along this harbour margin. It's anticipated that construction work for the entire project may, or may not, be done in stages, and it could be coordinated in the interests of financial forecasting and cost-efficiencies. Consequently, both elements are recognised in this report in order to determine a rough order of costs (ROC) for the entire pavement upgrade and culvert/drain remediation.

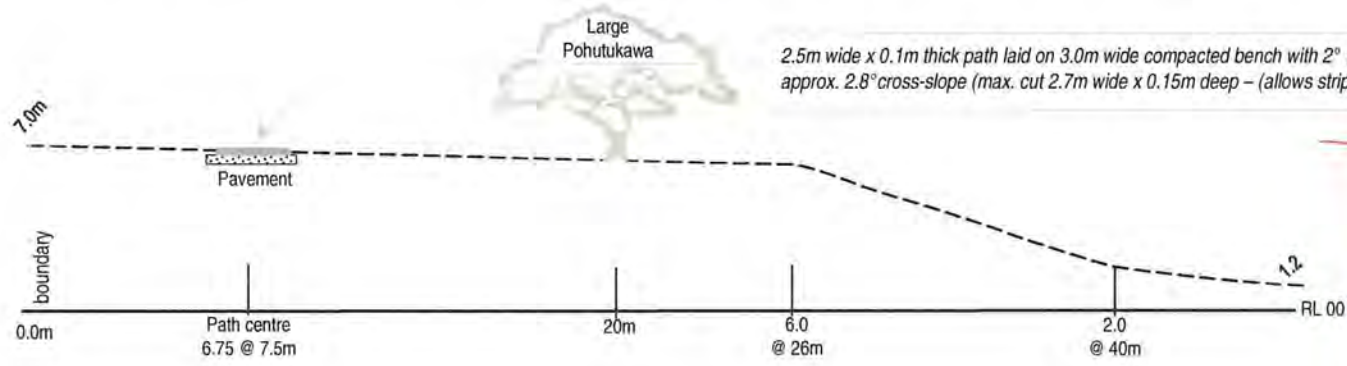
Note: the boxed comments down the left-hand margin of Sheet 3 outline four option considerations addressed separately in the resource consent process for remediation of the culvert scour problems. It is likely that Option 2 will be the likely outcome due to cost-optimisation and Regional Natural Resources Plan preferences.

SECTION 2.2 Earthworks Profiles & Estimated Volumes

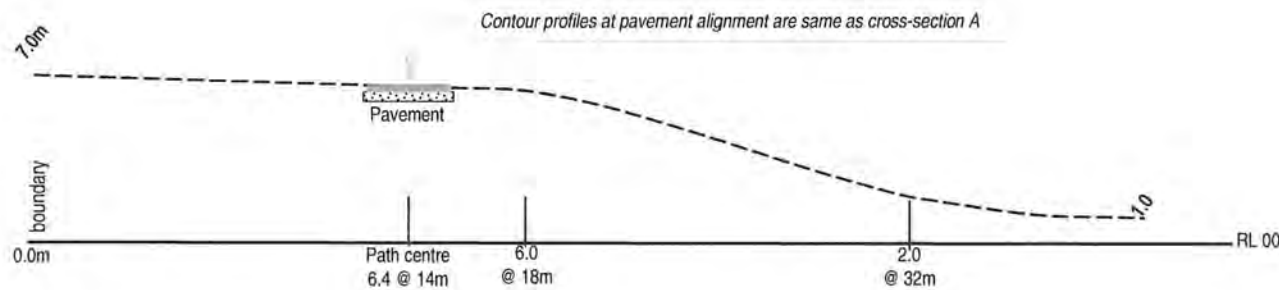
BENCH-LINE CROSS-SECTIONAL DRAWINGS – (refer sites on above sheets):

[Note – cross-sections are indicative only, being based on the land contours and on-site measurements at specific sites only.]

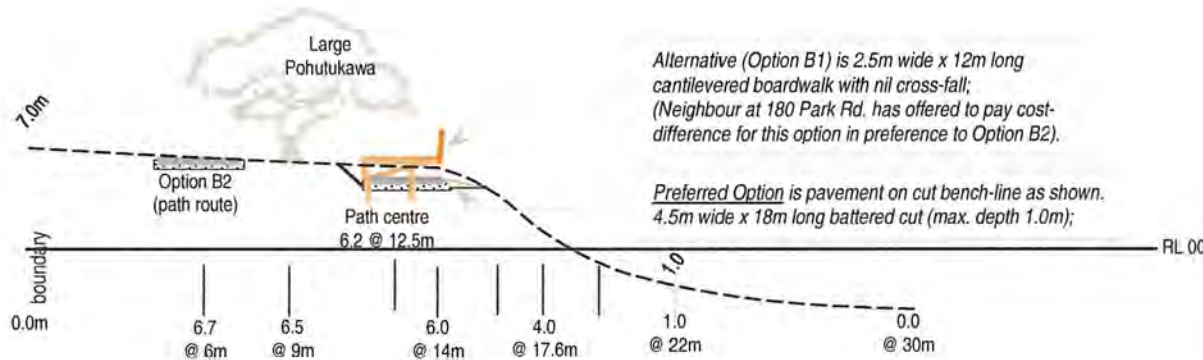
**Cross Section A**  
(at 55m from Park Rd)



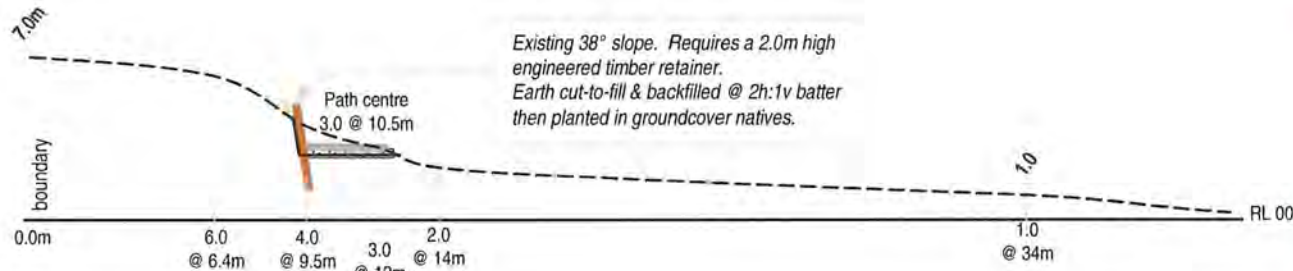
**Cross Section B**  
(at 93m from Park Rd)



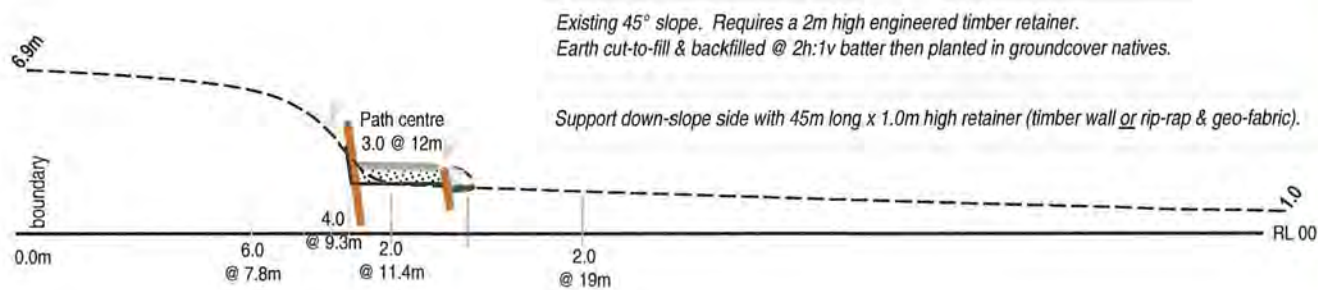
**Cross Section C**  
(at 112m from Park Rd)



**Cross Section D**  
(at 133m from Park Rd)

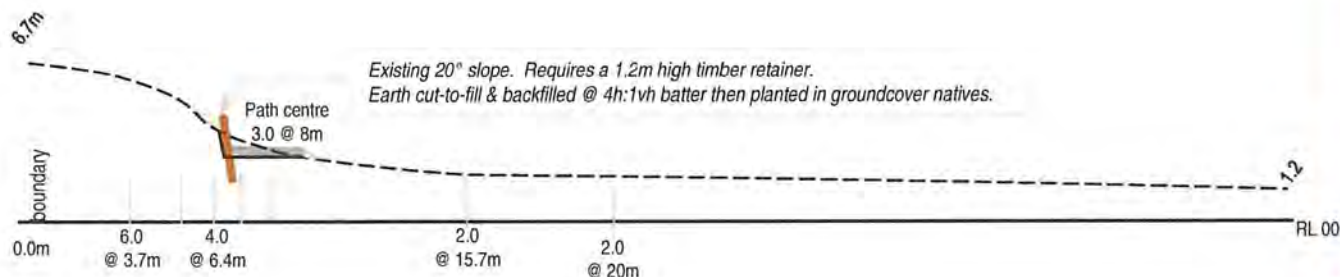


**Cross Section E**  
(at 159m from Park Rd)



distances\* = see note bullet-point (g) of "Methodology – General Notes" (p.12)

**Cross Section F**  
(at 181m from Park Rd)



**VOLUME 1**  
From chainage 0m to 112m.

**Methodology**  
Allow to remove up to 0.15m depth of topsoil, then lay & compact 0.05m depth of clay or metal sub-base as may be required, ready for laying 0.1m concrete pavement. 15% of excavated soil is to be re-used for edge dressing and balance to be used for infill elsewhere as required.  
The existing cross-fall between Park Rd and Cross-section 'C' is largely unchanged and can be averaged at 7%. Total earthworks in this sector is approx.:

**CUT** = 2.7m x 0.15m x 112m = 45m<sup>3</sup>  
**Plus**, Cross-section 'D' = 45m<sup>3</sup>  
**TOTAL CUT** = 90m<sup>3</sup>

**FILL**: allow 20% of cut topsoil for edge dressing = 18m<sup>3</sup>

**NET VOLUME** = 90m<sup>3</sup> cut less 18m<sup>3</sup> earth fill = 72m<sup>3</sup> **CUT**

**PLUS. Sub-base contingency**  
2.7m x 0.05 x 112 = 15m<sup>3</sup> sub-base (+25% compaction) = 19m<sup>3</sup>  
**19m<sup>3</sup> clay or metal sub-base to be compacted.**

**VOLUME 2**  
From chainage 112m to 181m.

This portion requires cut-to-fill. Total earthworks in this sector is approx.:

**Pavement Prep.**  
**CUT** = 1m x 0.75m x 69m = 52m<sup>3</sup>  
**FILL (solid)** = 3m x 0.75 x 69m = 155m<sup>3</sup>

**VOLUME A** = FILL less CUT plus compaction factor (i.e. 155m<sup>3</sup> FILL less 52m<sup>3</sup> CUT = 103m<sup>3</sup>)  
**PLUS** 25% (26m<sup>3</sup>) compaction = 129m<sup>3</sup>

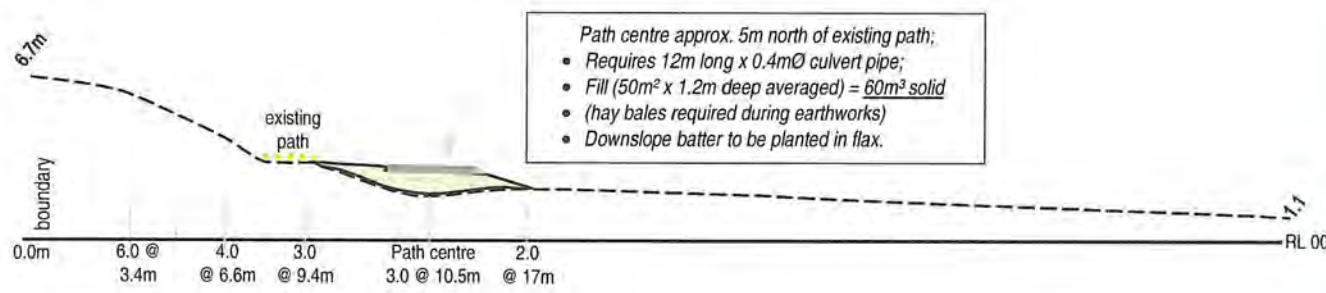
**Subtotal A** = 129m<sup>3</sup> **Loose FILL** (base spoil from project). **Plus**, 0.1m thick x (69m x 2.7m) = 18.5m<sup>3</sup> of clay or metal sub-base. All 147.5m<sup>3</sup> FILL to be compacted.

**VOLUME B** = (backfill & edge-dressing)  
**BACKFILL** (loose), behind upslope retainer @ approx. 0.75m<sup>3</sup> x 69m = 69m<sup>3</sup>  
**EDGE-DRESSING** allow 0.1m<sup>3</sup> x 69m = 7m<sup>3</sup>  
**Subtotal B** = 76m<sup>3</sup>

**NET SITE VOLUME**  
147.5m<sup>3</sup> + 76m<sup>3</sup> = 223.5m<sup>3</sup> **FILL**

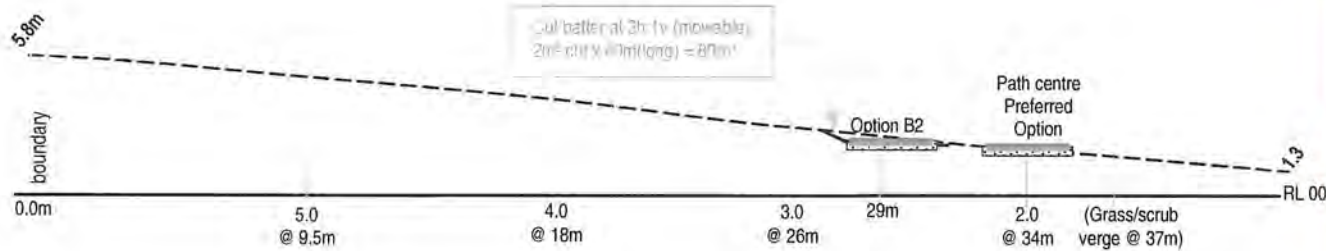
23

**Cross Section G**  
(at 186m\* from Park Rd)



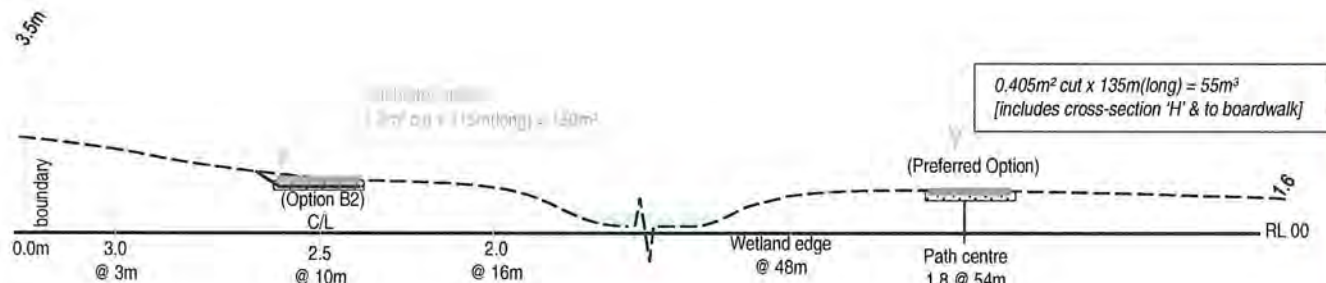
**VOLUME 3**  
 Fill site at Cross-section 'G'  
 This portion predominantly requires fill. Total earthworks in this sector is approx.:  
 CUT = Nil m<sup>3</sup>  
 FILL = 60m<sup>3</sup> (solid)  
 55m<sup>3</sup> Loose FILL (being base spoil from project) plus 25% (8m<sup>3</sup>) compaction = 63m<sup>3</sup>  
 Plus 0.1m thick x 50m<sup>2</sup> plus 25% compaction = 6m<sup>3</sup> of clay or metal sub-base.  
 All 77m<sup>3</sup> to be compacted)

**Cross Section H**  
(at 196m\* from Park Rd)



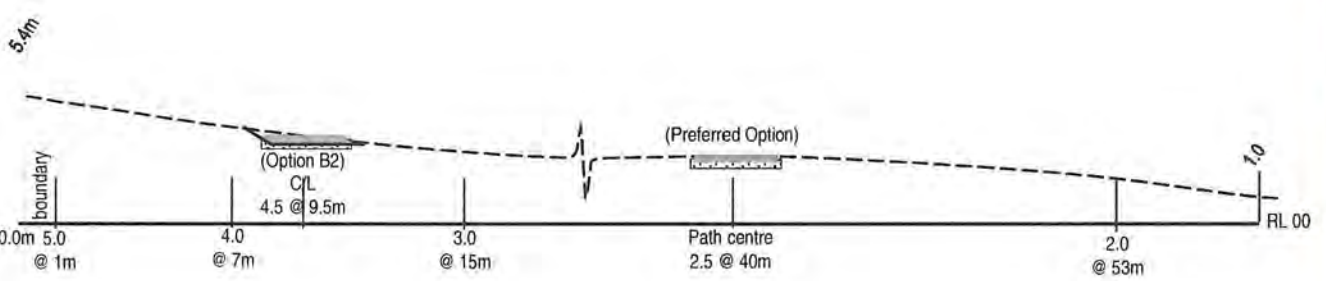
**VOLUME 4**  
 From chainage 196m to 301m.  
 Calculations are based only upon the route that's preferred in this report.  
 Methodology  
 Allow to remove up to 0.15m depth of topsoil as may be required, then lay & compact 0.05m depth of clay or metal sub-base, ready for laying 0.1m compacted metal capping or concrete pavement. 20% of excavated soil is to be re-used for edge dressing and balance to be used for infill elsewhere as and where required.  
 The existing cross-fall along most of the preferred route within Francis Dr. Reserve can be averaged at 7%.

**Cross Section I**  
(at 250m\* from Park Rd)

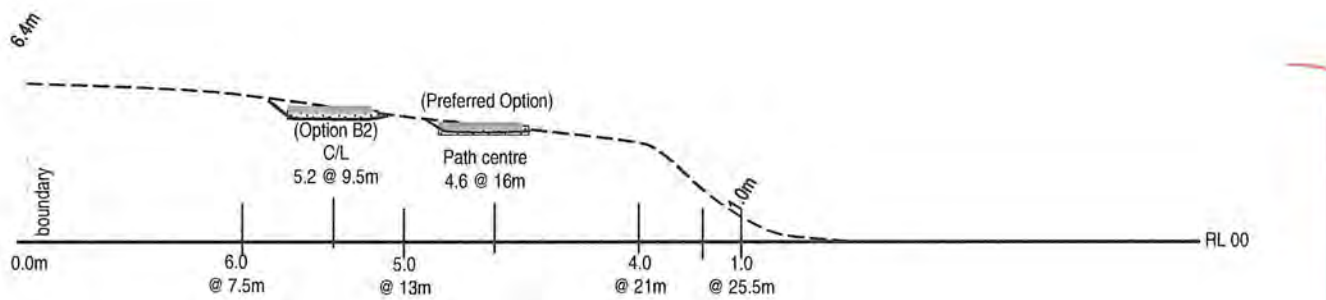


Total earthworks in this sector is approx.:  
 CUT = 2.7m x 0.15m x 105m = 42.5m<sup>3</sup>  
 FILL; allow 20% of cut topsoil for edge dressing = 8.5m<sup>3</sup>  
 NET VOLUME = 42.5m<sup>3</sup> cut less 8.5m<sup>3</sup> earth fill = 34m<sup>3</sup> Earth CUT  
 PLUS, Sub-base contingency  
 2.7m x 0.05 x 105 = 14m<sup>3</sup> sub-base (+25% compaction) = 17.5m<sup>3</sup>  
 17.5m<sup>3</sup> clay or metal sub-base to be

**Cross Section J**  
(at 301m\* from Park Rd)

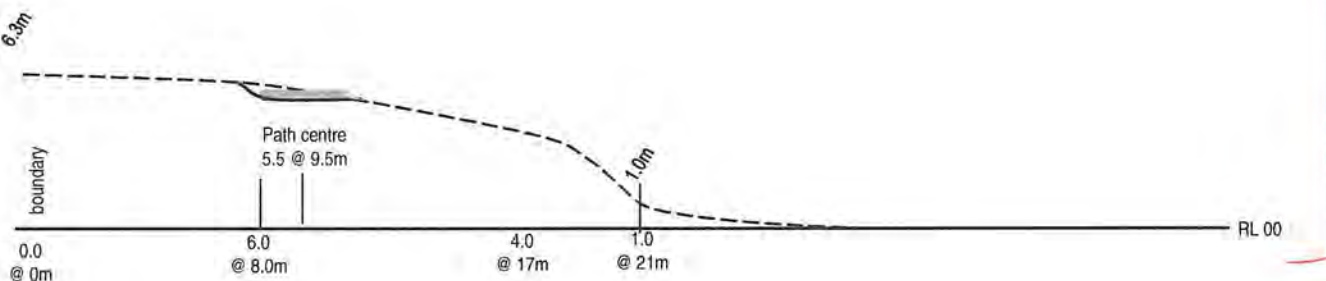


**Cross Section K**  
(at 333m\* from Park Rd)



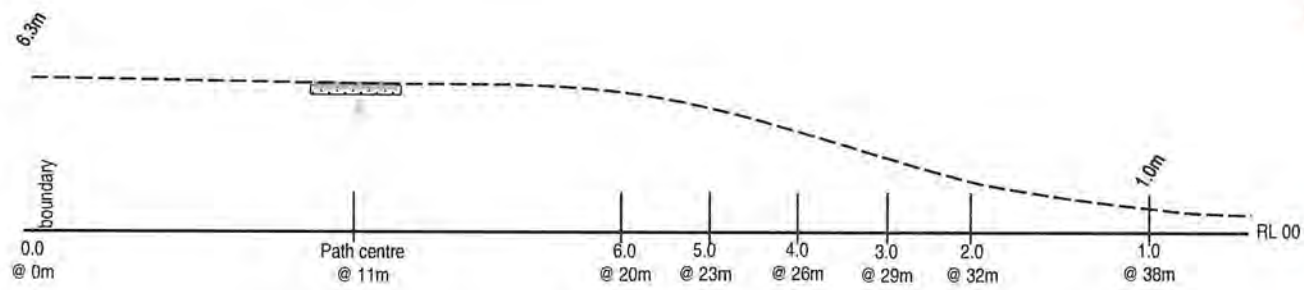
**VOLUME 5**  
 From chainage 301m to 347m approx.  
 Calculations are based only upon the route preferred in this report.  
 Methodology – cut per profiles & lay pavement on compacted sub-base (clay or metal as determined by site material). Total earthworks in this sector is approx.:  
 CUT = 1.7m x 0.3m (averaged) x 46m = 23.5m<sup>3</sup>  
 FILL = 1.0m x 0.1m (averaged) x 46m = 4.6m<sup>3</sup> (x25% compaction) = 6m<sup>3</sup>;  
 Plus, allow 2m<sup>3</sup> of won topsoil for edge dressing = 8m<sup>3</sup>  
 NET VOLUME = (23.5m<sup>3</sup> cut less 8.0m<sup>3</sup> fill) = 15.5m<sup>3</sup> Earth CUT  
 PLUS, allow Sub-base contingency of 2.7m x 0.05m x 46m = 6.2m (+25% compaction) = 8m<sup>3</sup> clay or metal sub-base to be compacted.

**Cross Section L**  
(at 347m\* from Park Rd)



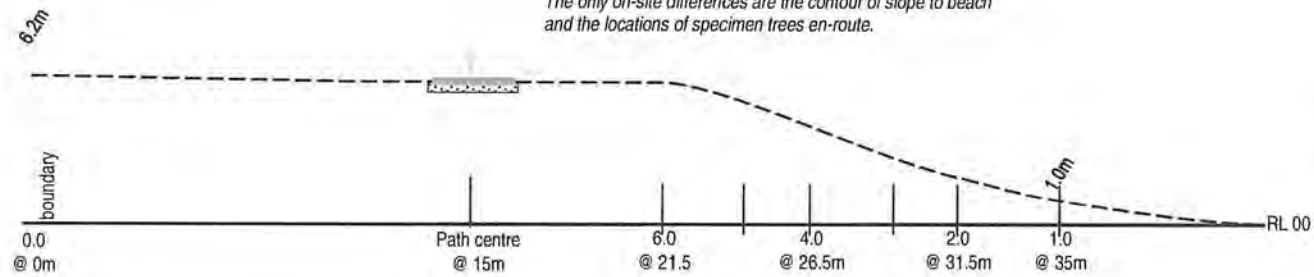


**Cross Section M**  
(at 462m\* from Park Rd)

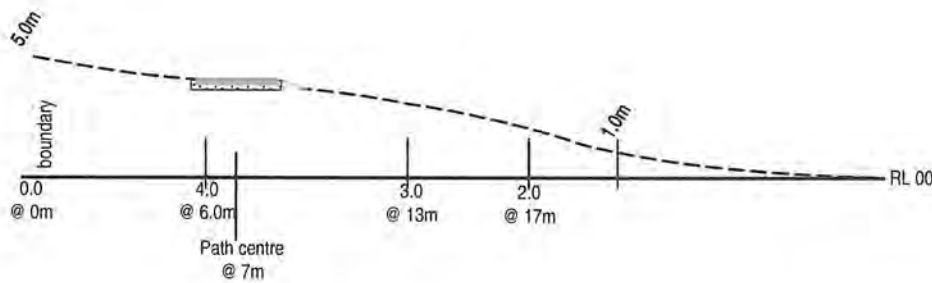


After cross-section 'L' the contour profiles at the pavement alignment are almost flat & are unchanged until cross-section 'O'. The only on-site differences are the contour of slope to beach and the locations of specimen trees en-route.

**Cross Section N**  
(at 560m\* from Park Rd)



**Cross Section O**  
(at 617m\* from Park Rd)



**VOLUME 6**

From chainage 347m to 654m (east end of boardwalk to Levley Lane Reserve). Methodology - same as Vol. 4 sector.

Total earthworks in this sector is approx.

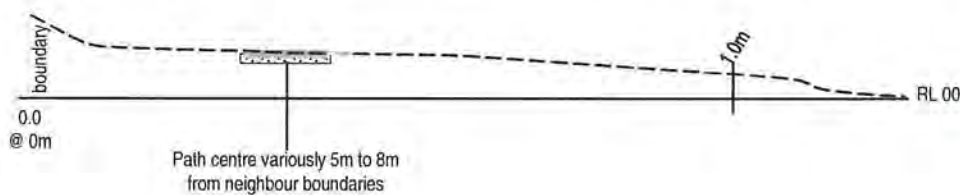
CUT = 2.7m x 0.15m x 307m = 124m<sup>3</sup>

FILL; allow 20% of cut topsoil for edge dressing = 25m<sup>3</sup>

NET VOLUME = 124m<sup>3</sup> cut less 25m<sup>3</sup> earth fill = 99m<sup>3</sup> Earth CUT

PLUS, Sub-base contingency  
2.7m x 0.05 x 307 = 41.5m<sup>3</sup> sub-base (+25% compaction) = 52m<sup>3</sup>  
52m<sup>3</sup> clay or metal sub-base to be compacted.

**Cross Section - Averaged from Levley Lane Reserve to the channel scour 55m before Stop-bank**  
(between 684m\* and 1,205\* from Park Rd.; plus, 45m path within Levley Ln. Res. to pump station access)



**VOLUME 7**

From chainage 684m to 1,205m (west end of boardwalk at Levley Lane Reserve to channel scour site near stop-bank). Plus 45m within Levley Ln. Res. to pump station access. Total distance = 566m. Total earthworks in this sector is approx.

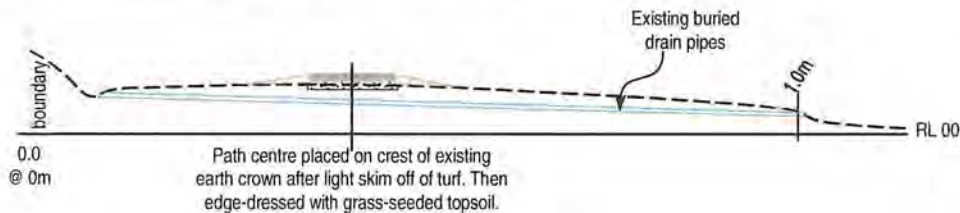
CUT = 2.7m x 0.15m x 501m (566 less 65m @ 62-66 Levley Ln.) = 203m<sup>3</sup>

FILL; allow 20% of cut topsoil for edge dressing = 41m<sup>3</sup>. Plus, 16m<sup>3</sup> topsoil edge dressing @ 62-66 Levley Ln. (41m<sup>3</sup> + 16m<sup>3</sup>) = 57m<sup>3</sup>

NET VOLUME = 203m<sup>3</sup> cut less 57m<sup>3</sup> earth fill = 146m<sup>3</sup> Earth CUT

PLUS, Sub-base contingency  
2.7m x 0.05 x 566 = 76.5m<sup>3</sup> sub-base (+25% compaction) = 95.5m<sup>3</sup>  
95.5m<sup>3</sup> clay or metal sub-base to be compacted.

**Typical Cross Section on esplanade reserve fronting 62 & 66 Levley Lane**



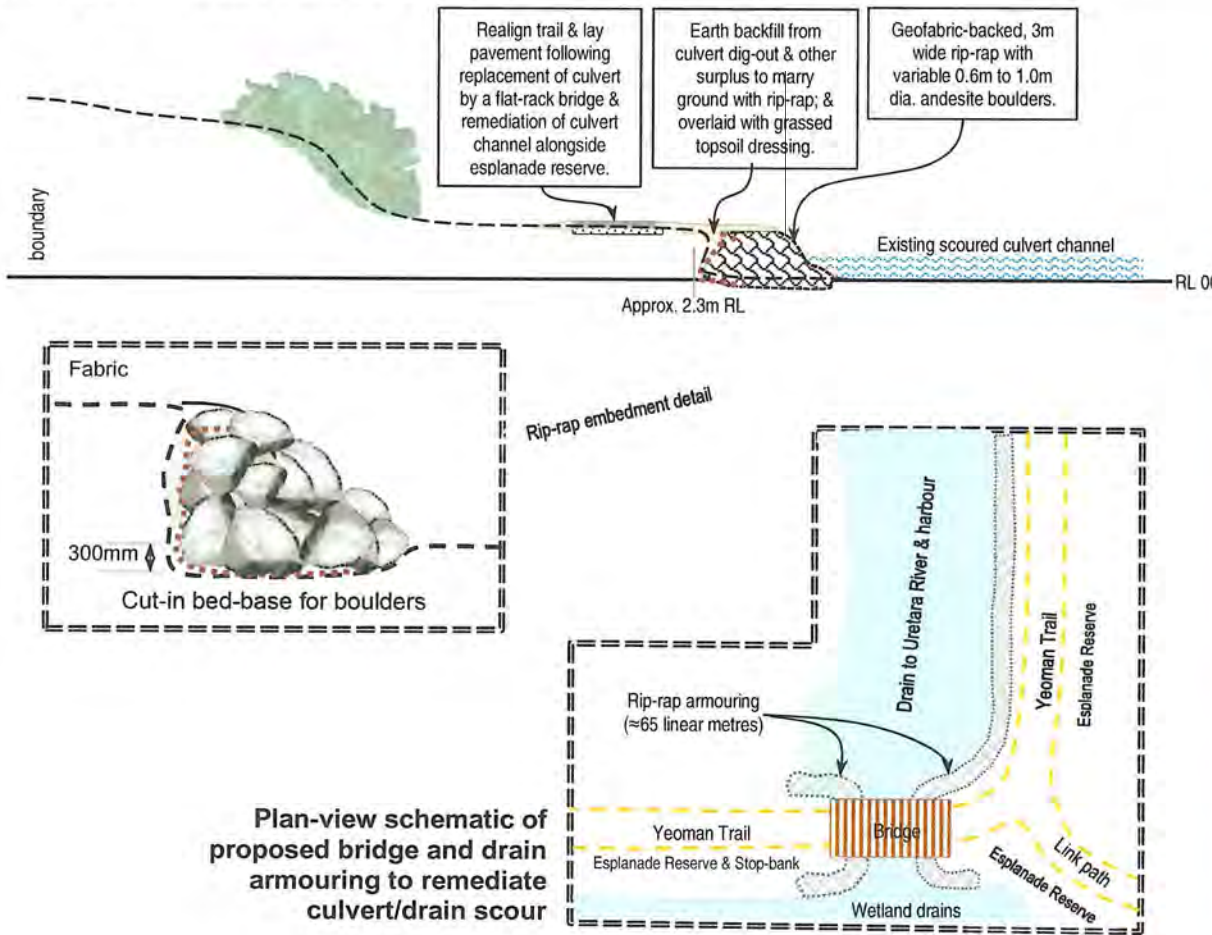
**METHODOLOGY - GENERAL NOTES & SPECIFICATIONS**

- Existing ground cross-falls average approx. 8% (10h : 0.8v).
- In most instances throughout the trail length from Park Rd. to the stop-bank the pavement is to be recessed 75% into existing ground so that pavement edges finish flush with surrounds after edge dressing. (Note: this will slightly elevate the path to prevent ponding & silts overlaying the paving whilst ensure the pavement doesn't act as an impediment to overland water flows.)
- After cutting-in the trail bed, any soft base (e.g. topsoil) shall be overlaid with compacted 50mm-deep clay or metal sub-base as necessary to achieve suitable preparation for finished pavement so as to avert slumping or concrete cracking, dependent upon finished surface.
- Soil won from recessing the pavement shall be utilised for edge dressing and for general fill & backfill as and where required.
- All raised pavement edges are to be dressed with grassed topsoil to marry into existing ground.
- Fronting 62 & 66 Levley Ln.; 65m of the ground is crowned; pavement along that portion is to be laid over the existing ground.
- Pavement Distances from Park Road exclude 45m of existing boardwalks. However, additional 45m of pavement is included to link to the vehicle accessway for the Utilities Dept. pump station within Levley Lane Reserve - (see Sheet 2 on p.8). Therefore, the total pavement distance within this specification is approx. 1,260m + 45m = 1,305m.

**Cross Section Z – Averaged alongside channel scour to stop-bank**

(Chainage approx. 1,260m to 1,305m from Park Rd) – refer photo 21 (p.6) & Sheet 3 map (p.9)

Illustrative treatment of 45m trail length to stop-bank. Work along this sector of the trail can only proceed in cooperation with BOPRC which is being coordinated alongside, but separately from, the balance of this trail upgrade evaluation.



**VOLUME 8**

From chainage 1,260m to 1,305m (alongside channel scour site to stop-bank). Total distance for pavement = 45m. Total earthworks in this sector extends to replacement of the culvert with a flat-rack bridge and channel-edge armoring & repair – includes proposed bridge abutment armour protection (total length of wrap-around armoring is 65m).

**CUT** = Nil for trail, but approx. 24m<sup>3</sup> from culvert removal.

**FILL** = Topsoil for site remediation & edge dressing, allow 30m<sup>3</sup>

**BACKFILL** between scoured bank edge & rock rip-rap = (0.3m x 2m x 50m) = 30m<sup>3</sup>

**NET VOLUME = 36m<sup>3</sup> FILL**

**OTHER – culvert replacement by bridge & rip-rap** (included in table below):

[This itemised cost tabulation is for ROC estimation purposes only]

Item	Details	\$ (Value) (estimated)
Non-woven Geotextile fabric AS640 or equivalent (e.g. A49 or 1160NP)		
Boulder rip-rap (0.7m to 1.0m dia.)		
Construct rip-rap armoring		
Clean-fill imported & laid		
Sundry allowance		
Culvert Earthworks + removal & disposal of culvert (Machinery & Labour)		
Flat-rack bridge		
Resource Consent application to BOPRC for works within CMA		
Pavement sub-base prep. & lay (all-in cost, using concrete option)		
	Subtotal	
Unforeseen	Contingency of 5%	
	<b>TOTAL</b>	

**SECTION 3. Scope of Work for Earthworks & General**

Based upon the information provided in Sections 1 & 2 above, the approximate volumes of earth movement necessary to prepare the pavement bench-line and accommodate all edge treatment are collated in this section. These estimates have been compiled to determine likely earthworks volumes for Heritage NZ authorisation purposes and to establish a rough order of costs for due financial consideration. This information will assist in compiling contract tender documentation should the decision be made to progress any, or all, parts of this report. These values are tabulated at Appendix III.

The volumes and costs take into account all works itemised within Sections 1, 2 & 3 of this document, being summarised as follows:

- 3.1 **Total approx. volume of earth CUT** (excludes the scoured esplanade reserve bank from chainage 1,205m to culvert):  
 $\approx [90m^3 + 52m^3 + 0m^3 + 42.5m^3 + 23.5m^3 + 124m^3 + 203m^3] = \mathbf{535m^3 \text{ total CUT volume.}}$
- 3.2 **Total approx. volumes of Uncompacted topsoil FILL** required for edge-dressing, compacted base & backfill:  
 $\approx [18m^3 + 205m^3 + 32m^3 + 8.5m^3 + 8m^3 + 25m^3 + 57m^3] = \mathbf{353.5m^3 \text{ total Uncompacted FILL volume.}}$
- 3.2 **Total approx. volumes of FILL (clay or quarry metal)** required for pavement sub-base:  
 $\approx [19m^3 + 18.5m^3 + 6m^3 + 17.5m^3 + 8m^3 + 52m^3 + 95.5m^3] = \mathbf{216.5m^3 \text{ total loose FILL volume (clay/metal) to bring onto site for Sub-base Compaction.}}$
- 3.3 **Variance between Cut & Fill Volumes** for redistribution within the project site (excludes the scoured esplanade reserve bank from chainage 1,205m to culvert):  
 CUT = 535m<sup>3</sup> (spoil won on-site)  
 FILL = [353.5m<sup>3</sup> on-spoil for side-dressing, retainer backfill & base-fill] + [216.5 of imported clay/metal for sub-base] = Total 570m<sup>3</sup> fill needed.  
 DIFFERENCE = 35m<sup>3</sup> more fill is needed than what is obtained from cuts (i.e. no on-site surplus).
- 3.4 **Culvert/bridge works & scour repair zone** (fill & topsoil):  
 After re-using the soil won during the culvert removal, the difference of cut to fill is 6m<sup>3</sup> deficit for backfill & compaction behind rip-rap armoring. Also, 30m<sup>3</sup> topsoil required for edge dressing.

**3.5 Retaining Walls:**

Description	Materials	Construction details	Estimated Costs
1m high x 50m long	<ul style="list-style-type: none"> <li>• 38 timber poles (2.5m long, 0.2m SED, H4)</li> <li>• 250 linear m. of Timber (200x50mm, RS, H4)</li> <li>• Nails (100mm galv.)</li> <li>• Geotextile (AS440, A49 or equiv.)</li> <li>• Drainage metal (0.2m<sup>3</sup> per running metre)</li> </ul>	<ul style="list-style-type: none"> <li>• Poles placed 1.4m in-ground @ 1.2m centres with 4° rake.</li> <li>• 2 nails per post for all timbers.</li> <li>• Full wall-depth geotextile backing.</li> <li>• 10m<sup>3</sup> of metal placed behind base of wall.</li> </ul>	
2m high x 60m long	<ul style="list-style-type: none"> <li>• 50 timber poles (4.2m long, 0.2m SED, H4)</li> <li>• 600 linear m. of Timber (200x50mm, RS, H4)</li> <li>• Nails (100mm galv.)</li> <li>• Geotextile (AS440, A49 or equiv.)</li> <li>• Drainage metal (0.3m<sup>3</sup> per running metre)</li> <li>• 65m Novacoil (50mmØ, punched &amp; fabric wrapped)</li> </ul>	<ul style="list-style-type: none"> <li>• Poles placed min. 2.2m in-ground @ 1.2m centres with 4° rake.</li> <li>• 2 nails per post for all timbers.</li> <li>• Full wall-depth geotextile backing.</li> <li>• 18m<sup>3</sup> of drainage metal.</li> <li>• Novacoil outlet directed beneath trail to seaward side of lower wall.</li> </ul>	
Batter above	<ul style="list-style-type: none"> <li>• Coconut matting, pins, native plants, fertiliser tabs</li> </ul>	<ul style="list-style-type: none"> <li>• Matting pinned &amp; plants with fertiliser</li> </ul>	

Labour & Machinery to construct both walls			
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**3.6 Site Preparation:**

- 1) The total pavement length to the stop-bank culvert is 1,305m as specified within this documentation – this includes the 85m trail diversion within Levley Lane Reserve. The existing grass growing within the pavement alignment will need to be mown by Council’s reserves maintenance contractor prior to construction work commencing.
- 2) Removal of vegetation on batters which need to be retained, infilled and/or backfilled, as well as pruning of tree limbs as may be necessary, and off-site disposal or green-waste would form part of any contract works. An arborist approved by Council would need to be used when pruning any ornamental trees, identified in this report and resulting chip from this arboricultural work should be salvaged for appropriate on-site use of the chip as mulch.
- 3) Appropriate silt management will be needed between the potential project zone and Tauranga Harbour to prevent any sediment entering the harbour during the construction work. Although resource consent for this project (culvert site excepted) is not required, any construction contractor will need to operate within the sediment management and earthworks protocols as detailed at Rule LM R1 of the Regional Council’s Natural Resources Plan – a copy of that Rule will need to be included into any potential construction contract.

**3.7 Compacted Metal-finish Specification – (in event of metal-capped finish):**

- 1) Satisfactory ground compaction must follow each 0.5m depth of fill.
- 2) The sub-base shall have a minimum 100mm thickness of acceptable crushed-rock (metal) capping spread over. This shall comprise material in the order of 15mm to 40mm “all-in” and may consist of GAP-20, clean crushed concrete, or other similar-sized and durable material approved by Council.
- 3) The capping metal shall be graded by back-blade or machine bucket to produce a general smooth surface with a minimum of 2° crown, or cross-fall as required, prior to rolling.
- 4) The metal capping shall simultaneously be dampened and compacted by a minimum of four-passes with a two-tonne vibrating-roller to produce a final compacted surface. Further metal dressing, grading and rolling may be required to achieve a regular, smooth and professional finish with a compacted capping depth of not less than 60mm.
- 5) The final pavement surface shall be approx. 1,305m long and not-less-than 2.5m wide (with the exception of 15 liner metres of pavement at cross-section ‘D’ which shall be 2.0m wide) and must generally have a crown or cross-fall of approx. two degrees.

**3.8 Concrete Pavement Specification – (in event of concrete finish):**

Satisfactory ground compaction must follow each 0.5m depth of fill. Also, the final surface preparation must be compacted by a minimum of four passes using a two-tonne vibrating roller prior to laying the concrete pavement. The final pavement sub-base surface shall be approx. 2.7m wide and must generally have a cross-fall of less than two degrees.

To satisfy the intended end-uses and to comply with the standard residential street pavement requirements shown in the table below, the foot/cycle path shall be:

- 1) Approx. 1,305 metres long (give-or-take 10 metres);
- 2) 2.5m wide;
- 3) 0.1m thick;
- 4) Strength – Concrete mix shall conform to 20 MPa grade;
- 5) Un-reinforced with 25mm deep contraction cuts/score-lines across the full pavement width at 3m spacing;

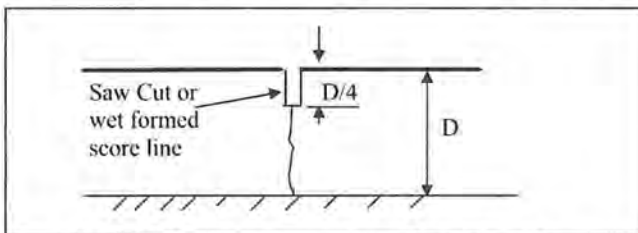


Fig.1 Control joint in an un-reinforced footpath slab.

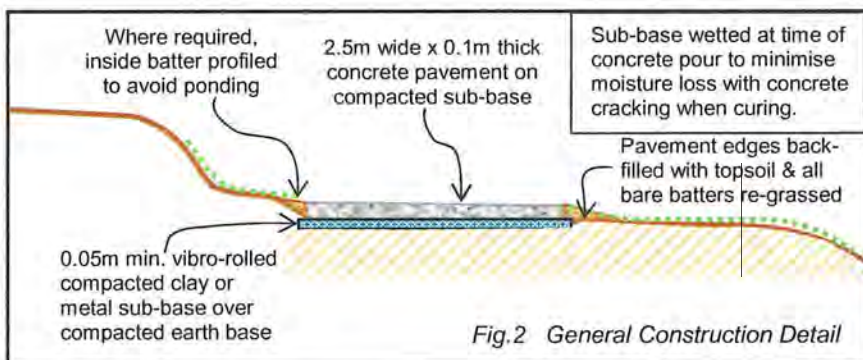


Fig.2 General Construction Detail

**3.9 Boulder rip-rap retaining & culvert replacement with Council supplied bridge**

Between chainage 1,260m and 1,305m (alongside the channel scour near the stop-bank culvert) there are specific works which will require resource consent and that consent application is simultaneously being compiled. The issues are briefly described at the following references herein:

- Sec.1 (photo 21)
- Sec.2.1, Sheet 3, Cross-section ‘Z’
- Sec.2.2, Cross-section ‘Z’ & its accompanying table.

**3.10 Archaeological Authority Requirements:**

All works will be required to comply with a Heritage NZ “Authority to Modify” which is yet to be sought. Such will follow an archaeologist’s evaluation report. An archaeologist has not yet been charged with providing the evaluation report.

**3.11 Health & Safety – Partial reserve closure:**

In the interests of public safety, it’s likely that sections of the esplanade reserve will need to be systematically closed to public access during trail construction work. Where sufficient esplanade reserve width permits, redirection notices and fencing could allow the public to walk around construction workers thereby minimising the duration of esplanade reserve closure to pedestrian through-traffic. However, some sites (viz. from cross-sections ‘C’ to ‘H’, and the culvert site) do not permit deviations so the esplanade reserve will need to be closed during work at those sites. It’s anticipated that the total project work would variously impact public utilisation of this esplanade reserve network for up to four months from commencement to completion of the job.

**3.12 Site Finishing & Clean-up:**

- 1) Slope batters would be contoured with topsoil to marry in with the natural ground-form and the concrete pavement edges shall be back-filled with topsoil to merge with the adjacent contoured batters.
- 2) Formed batters and pavement edges shall be grass-seeded using amenity turf rye grass.

- 3) All work-site materials shall be removed at the completion of works to leave the site in a tidy state.
- 4) Steep cuttings (for instance) above the retaining walls will need to be overlaid with biodegradable coconut matting – pinned in place – and planted with native shrubbery (see Photos 5 & 6, p.3).

## SECTION 4. Rough Order of Costs & Pricing Schedule

- An Excel spreadsheet, for financial forecasting and potential contract tendering purposes, is provided separately but is to be read in conjunction with this report. A copy of that spreadsheet is included at Appendix III of this report.
- **It is anticipated that this investigation report may be made available to the public in the process of further community consultation. Therefore, for reasons of commercial sensitivity (viz. potential project tendering purposes) it is anticipated that Appendix III may be redacted to the public. For the same reason, the green-letter texts which appear in the two tables at p.13 of this report may need to be redacted.**

## Appendices

### Appendix I. Resource Consent Considerations

#### 1. Regional Council – Regional Natural Resources Plan (RNRP)

Although this activity is within the coastal environment (i.e. within 20m of mean high-water spring), it does not trigger any Bay of Plenty Regional Council consent requirements so long as there is compliance with standard permitted activity rule conditions regarding sediment management and earthworks protocols, as detailed in LM R1 of the Regional Natural Resources Plan as follows:

*LM R1 (Rule 1) Permitted – Earthworks and Quarries* The disturbance of land and soil as a result of earthworks or a quarry, where the activity does not exceed the limits in Table LM 1 within any 12-month period is a permitted activity subject to the following conditions:

- There shall be no point source discharge of sediment contaminated stormwater to surface water from the activity.
- The diffuse discharge of sediment contaminated stormwater to surface water from the activity shall not cause the following effects, except where a 20% AEP flood event is exceeded:
  - The production of any conspicuous oil, grease films, scums or foams, or floatable or suspended solids.
  - Any conspicuous change in colour or visual clarity.
  - Any emission of objectionable odour.
  - The rendering of fresh water unsuitable for consumption by farm animals.
  - Any more than minor adverse effects on aquatic life.
- The activity shall not cause or induce erosion to land or to the bed or banks of any surface water body, where the erosion is persistent or requires active erosion control measures to bring it under control. Erosion includes: (i) Instability of land or the banks of the surface water body. (ii) Scour to the bed of the surface water body
- Fill from the earthwork activity shall not be deposited in overland or secondary flow paths that convey stormwater during rainfall events.
- The activity shall not obstruct or divert the flow of water in such a manner that it results in damming, flooding or erosion.
- The activity shall not disturb vegetation in a wetland; or change the water flow or quantity, or water quality in a wetland.
- Where an activity is a cleanfill site, the activity shall comply with the Ministry for the Environment's Cleanfill Guidelines (2001)14.
- The activity shall not disturb an identified contaminated site.
- No machinery refuelling or fuel storage shall occur at a location where fuel can enter any water body.
- No contaminants (including, but not limited to, oil, hydraulic fluids, petrol, diesel, other fuels, paint, solvents or anti-fouling paints), excluding sediment, shall be discharged to water, or discharged to land in circumstances where the contaminant may enter water, from the activity.
- All practicable measures shall be taken to avoid vegetation, soil, slash or any other debris being deposited into a water body or placed in a position where it could readily enter or be carried into a water body.
- The activity shall be staged, managed and completed, and the activity site closed-off, in a manner that ensures compliance with conditions (a) to (k) inclusive.
- Any stormwater from outside the exposed area shall be kept separate from the earthworks area.
- Where the earthworks are for stream crossing purposes, the activity shall also comply with the following conditions:
  - The crossing shall be made at, or near to, right angles to the flow of the water in the river or stream, ensuring minimal roading in the Riparian Management Zone.
  - The area shall be stabilised as soon as practicable, but no later than 3 months from the end of the activity.
  - All practicable steps shall be taken to keep stormwater away from the stream crossing approach.

Table LM R1 (Rule 1) Permitted – Earthworks and Quarries; specifies the permitted limits for earthworks within any 12-month period. Within the Coastal Margin (sec.(g) of the table) these limits are exceeded by the proposed trail activity; therefore, they would normally fall to be considered as a discretionary activity under LM R4, except the definition of earthworks precludes this.

#### Table LM R1 Notes:

*"Any earthworks: (a) In the Erosion Hazard Zone, or (b) On slopes greater than 35 degrees; or (c) On coastal land between 0-50 metres of the Coastal Marine Area on Sand Dune Country; or (d) On coastal land between 0-20 metres of the Coastal Marine Area on the Coastal Margin; are discretionary activities under LM R4.*

*The area covered by Table LM 1(a) will be interpreted to be modified or stabilised Sand Dune Country which has a vegetative cover, sealed or compacted soil, and a previously modified or flattened topography. This excludes unmodified or natural dune systems.*

*Any earthworks in the Coastal Margin between 0 to 20 horizontal metres as measured from the Coastal Marine Area on the edge of an estuary, harbour, or the open rocky coast are a discretionary activity under LM R4."*

#### **IMPORTANTLY:**

The RNRP definition of "Earthworks" within the "Definition of terms (updated 1 may 2018) is:

*"Earthworks – Any activity that exposes, disturbs, places or deposits land and soil. Such activities include, but are not limited to, tracking, roading, cleanfill sites, cut and fill operations, quarrying, mining, and recontouring. Excludes area-wide treatment (pavement overlay and strengthening) and road resealing (pavement rehabilitation) of existing roading, normal domestic gardening practices, maintenance of roads and tracks (including railway tracks), the formation of walking tracks, cultivation (except where re-contouring is involved), maintenance of linear network utility support structures, and maintenance (including minor realignment) of existing foot tracks within public reserves and the conservation estate."* (underlines added for this investigation report.)

**SUMMARY: The proposed activity does not come under the definition of "Earthworks"; therefore, so long as the standard permitted activity rule conditions [LM R1 (a) to (n)] are complied with during the on-site project activity then consent from BOPRC is not required. N.B. This resource consent conclusion does not address the culvert replacement & scour repair within the CMA – it's understood that matter is being worked through between WBOPDC & BOPRC staff.**

2. District Council – District Plan (DP)

District Plan (DP) Objective, Policies & Rules	Response
4B. Transportation Policy 8. To ensure land use, development and subdivision planning provides for the implementation of multi-modal transport activities including public transport, walking and cycling facilities that address the identified need for new facilities/networks or enhance existing facilities/networks.	The activity is consistent with and supports the Issues, Objectives & Policies of integrated, multi-modal transport. Being cognisant of Katikati's rapid growth, the proposed activity anticipates increased, year-round, community and visitor pressure upon the existing reserves access network as a desirable alternative to vehicular traffic.
<u>5. Natural Environment</u> – (includes Significant Ecological Features as detailed in Appendix 1 of the DP.) 5.4.1 Permitted Activities include: (d) Activities on reserves as provided for in the Reserves Act 1977 and the Conservation Act 1987.	The activity is within reserves administered by Council and the activity is generally anticipated by the Reserves management Plan; therefore it is a permitted activity in respect to Sec. 5 of the District Plan (DP).
<u>6. Landscape</u>	The activity sites are within the urban zone and are not within any S8 feature (within 50m of Tauranga Harbour) as identified in the DP; therefore, Sec. 6 of the DP is not applicable.
<u>8. Natural Hazards</u> 8.2.1 Objectives 1. Minimisation of the risk of natural hazards to human life and the natural and built environment. 2. Protection of the existing natural character of the coastal environment and other natural features having recognised ecological, landscape or other significance to the District.	The activity does not present, or raise the risk of, natural hazards to human life or to the existing natural & built environment. The reserve sites through which the activity shall pass are not in a natural state; therefore the activity does not alter any natural character or natural features of significance to the District.
8.2.2 Policies	Response
1. Adopt the best practicable options (including the 'do nothing' option) in the management of areas actually or potentially at risk from natural hazards and where possible adopt avoidance rather than mitigation or remedial measures.	The potential natural hazards to which the activity could be exposed are: (i) A short length (approx 40m) of potential slope instability; whereby, the proposed activity will improve slope stability; (ii) Floodable ground – the proposed activity will provide all-weather access except in extreme event, whereas this is not the existing case. Also, the structure will not be adversely affected by a rare flood event and will not adversely affect neighbouring overland flow patterns.
2. Control or prevent the establishment of activities which have the potential to increase the extent to which natural hazards have or may have an adverse effect on human life or the natural or built environment.	See response to 8.2.2.1 above.
3. Enable the development or redevelopment of land already subdivided or otherwise developed for urban purposes in areas now known to be at risk from natural hazards only where any likely adverse effects can be avoided or appropriately mitigated.	See response to 8.2.2.1 above.
4. Ensure that new subdivision, land use activities or other development is located and designed so as to avoid the need for further hazard protection works.	See response to 8.2.2.1 above.
5. Ensure that where hazard protection works are necessary their form, location and design are such as to avoid or mitigate potential adverse environmental effects.	No specific hazard protection works are intended.
6. Enable natural ecosystems in currently undeveloped areas to migrate inland as a result of dynamic coastal processes (including sea level rise as predicted by recognised national or international agencies).	The proposed activity is not within an coastal risk site, as identified in the DP, and will not obstruct natural processes.
7. Encourage the conservation and enhancement of natural features such as sand dunes and wetlands which have the capacity to protect existing developed land.	No natural dune, wetland or ecological features are compromised by this proposed activity.
8. Prevent the use of concrete and block work foundations, floors and walls in the Coastal Erosion Areas.	The activity is not within an identified Coastal Erosion Area.
<u>8.3.1 Permitted Activities (c) Floodable Areas</u> (i) Buildings/Structures where evidence establishes: - A building/structure will be located clear of the floodable area irrespective of the extent of the floodable area shown by the Planning Maps; or - A building/structure will not be affected by the floodable area. <u>Explanatory Note</u> Suitable evidence may include, but is not limited to, aerial photographs, site inspections from Council engineers, and engineering assessments from a suitably qualified person. 8.3.2 Controlled Activities – N/A 8.3.3, 8.3.4. & 8.3.5 Restricted Discretionary Activities – N/A. <u>8.5 Matters of Discretion</u> 8.5.1 Restricted Discretionary Activities 8.5.1.3 Floodable Areas and Coastal Inundation Areas (a) The effect of the proposed activity (including its location and design) on the capacity of ponding areas and function of overland flow paths. (b) The appropriate minimum finished floor level of the proposed building/structure.	A concrete or metal-paved walk/cycleway could be defined as a structure. If the pavement is concrete then it will not be affected by any floodable zones that it passes through. If the pavement is metal-surfaced it can be scoured out by severe flood events at specific sites. If the surface remains in grass it will require periodic repairs (as presently occurs) but the level of surface damage will increase during wet months as public use intensifies over time. Re. 8.5.1.3(a): Since the structure will not materially protrude above existing ground, and most certainly will not obstruct overland flowpaths to or from neighbouring properties, it will not present adverse flood-related or ponding effects to neighbouring properties. This matter needs liaison with Council's Planning staff to confirm that the path will not affect overland flow patterns to the detriment of neighbours or the environment, and to confirm that any part of the path within a floodable zone will not be adversely affected.
<u>13. Residential</u> – Policy 13.2.2.6 Provide safe, usable and attractive networks and associated linkages for pedestrians, cyclists and motor vehicles. <u>13.3.1 Permitted Activities</u> – 13.3.1(e) Public works and network utilities as provided for in Section 10.	The activity aligns with Policy 13.2.2.6 and is not inconsistent with Section 10 of the DP respecting public works and utilities.
<b>Summary: Resource consent is unlikely to be required. The proposed activity will ensure all-weather/all-season pedestrian &amp; cycle access along the reserves network. It will marginally impact the existing visual 'greenspace' appeal of the reserve access network but with the alignments chosen the extent of visual impact is not excessive. That is not a resource consent matter. In any event, retention of all the visual greenspace (existing mown grass) is secondary to the purposes of esplanade and recreation reserves as defined by the Reserves Act 1977 and addressed in the operative reserves management plan. The activity is supported by DP Policies 4B.8 &amp; 13.2.2.6. Clarification from Planning staff is required re. Policy 8.3.1 and the associated Explanatory Note (see above), coupled with the response here-given in the right-hand column above.</b>	

Appendix II. Pavement Options & Rationale

1) The Reasons for All-weather Pavement

Policy 6.82.5 of the Katikati Ward Reserves Management Plan states: "Develop a walkway/cycleway between the Yeoman Walkway, Riverlea Drive Reserve, Levley Lane Reserve, Francis Drive Reserve and Park Road Reserve next to the retirement village." The rationale behind that policy is:

- a) A recently released study (utilising 37 years of census data) places Katikati amongst the top eight towns in New Zealand for liveability and livelihood. This recognises the climate, natural & social environment, commercial and educational opportunities, employment levels, property values, transportation network, natural and public amenities, and the opportunities for growth and development. It is reflected in the steady rate of population and allied community growth.
- b) With community growth comes increased pressure on resources resulting in wear-&-tear of existing facilities, coupled with increased expectations for appropriate urban standards of facilities – what may once have been an acceptable standard in a small rural town is no longer adequate for increased numbers of users with urban lifestyles. Within Katikati, and at Omokoroa, Plummers Point, Te Puke, Pukehina and Waihi Beach, new and improved pathways have been built in tandem with these community drivers. This "Yeoman Walkway to Park Road" trail upgrade is a continuation of that process. It also aligns with multi-modal community access and transport planning objectives.
- c) The issues and options associated with this proposed trail upgrade are listed and compared in the following table:

Options – Mown Grass, Compacted Aggregate & Concrete

Issue	Mown Grass	Compacted Aggregate	Concrete Pavement
Cost comparison	<ul style="list-style-type: none"> <li>• Trail requires upgrade from chainage 112m to 200m, plus 92m of retaining walls, infill &amp; realignment (≈\$46,000).</li> <li>• Plus remediation of scour zone &amp; culvert replacement from chainage 1,205m to 1,260m @ ≈\$88,000.</li> <li>• Plus 50% of Fees</li> <li>• Total = \$149,000</li> </ul>	<ul style="list-style-type: none"> <li>• Trail requires same costs per Mown Grass option).</li> <li>• Plus same site prep. (cut/fill).</li> <li>• Plus adequate compacted aggregate fill.</li> <li>• Plus 10-yearly maintenance repairs.</li> <li>• Total = \$340,000</li> </ul>	<ul style="list-style-type: none"> <li>• All as summarised at Appendix III; Total = \$470,000</li> <li>• [\$321,000 more than grass]</li> <li>• [\$130,000 more than compacted aggregate metal finish]</li> <li><b>NB:</b> Overall cheaper lifecycle cost.</li> </ul>
Surface texture & hazards	<ul style="list-style-type: none"> <li>• Three zones currently have grass worn to earth year-round; two other (longer) zones are wet in winter months.</li> <li>• Ground is unpleasant for street foot-wear (wet feet) during all times of wet or dew-laden grass – limits useful times.</li> <li>• Surface is slippery with hazard potential for cycles.</li> <li>• Surface is not ideal for 'mopeds', especially in wet or winter states.</li> <li>• Limits district, regional &amp; national planning objectives for multi-modal transport re. providing alternative, safe, usable &amp; attractive networks and associated linkages for pedestrians, cyclists and motor vehicles.</li> </ul>	<ul style="list-style-type: none"> <li>• Addresses most wear-&amp;-tear and wet-ground issues.</li> <li>• Provides all-weather use of the trail.</li> <li>• Prone to surface damage &amp; scour within floodable zones.</li> <li>• Surface can become rutted due to rain-splash/scour, birds dust-bathing &amp; user traffic; also, some sites will become prone to aggregate wash/run-off. These effects require periodic surface repairs with additional fill &amp; compaction.</li> <li>• Surface can be slippery with hazard potential for cycles.</li> <li>• Provides road-traffic alternative for safe, usable &amp; attractive networks and associated linkages for pedestrians, cyclists, wheelchairs &amp; mobility scooters.</li> </ul>	<ul style="list-style-type: none"> <li>• Wear-&amp;-tear issues are resolved.</li> <li>• Provides year-round and all-weather use of the trail.</li> <li>• Stable, non-slip, durable and relatively maintenance-free surface.</li> <li>• Provides road-traffic alternative for safe, usable &amp; attractive networks and associated linkages for pedestrians, cyclists, wheelchairs and mobility scooters.</li> </ul>
Dust, mud & noise	<ul style="list-style-type: none"> <li>• Presently there are minimal off-site dust &amp; mud issues, but over time this is likely to increase at some sites due to increased wear-&amp;-tear of turf, resulting in increased repairs &amp; maintenance service requests to Council.</li> </ul>	<ul style="list-style-type: none"> <li>• In prolonged dry &amp; windy conditions dust from the compacted aggregate could be an irritant to some neighbours (N.B. on the stop-bank, which has compacted aggregate, there are no neighbours to be affected by dust). Mud would not be an issue.</li> <li>• Cycle tyres can be heard on the aggregate – this might be a minor irritant to some neighbours; similarly, increased mobility scooter use is likely.</li> </ul>	<ul style="list-style-type: none"> <li>• Dust &amp; mud issues will not exist.</li> <li>• Cycle tyres &amp; mobility scooters can be heard on the pavement, but will be quieter than on aggregate.</li> <li>• Increased, year-round &amp; all-weather cycle, pedestrian &amp; mobility scooter use is expected on a paved trail. This will be typically accompanied by increased vocal noise, but that is consistent with urban growth and with the principal purpose for esplanade &amp; recreation reserves.</li> </ul>
Ongoing maintenance cost	<ul style="list-style-type: none"> <li>• Reserves maintenance mowing costs are allied to surface area and mowing frequency, so the 3,150m<sup>2</sup> area occupied by the pavement will be deducted from the maintenance contract with corresponding annual savings.</li> <li>• Repairs – by resurfacing worn sites with topsoil &amp; grass, or aggregate or concrete – will not be substantial but will be a periodic, and increasing cost over time. Eventually, some segments of the trail will need treatment other than mown grass.</li> </ul>	<p>Periodic (approximately three-yearly) maintenance will be required to remedy side-wash and attend surface repairs.</p>	<p>Aside from edge-cutting of grass by the reserves maintenance contractor, ongoing maintenance will be almost non-existent for the foreseeable future.</p>
Edge management & Aesthetics	<ul style="list-style-type: none"> <li>• The mown reserves network along this harbour margin, with its trees, has an open park-like feel along much of its length. For some neighbours it also serves as an adjunct to their unfenced property 'frontages'. This gives an aesthetic appeal for neighbours and for reserve users, but over time some sites will succumb to worn earthen paths through the grass.</li> <li>• Some neighbours may wish to keep the trail in a minimalist form so as to keep user levels constrained along the reserves network, rather than for solely aesthetic reasons.</li> </ul>	<ul style="list-style-type: none"> <li>• Management of vegetation overgrowth would be required and would likely be done by herbicide treatment, which will be objectionable to some neighbours and trail users. Herbicide use along the trail edges would also adversely, and noticeably, affect the pleasant aesthetic of these reserves.</li> </ul>	<ul style="list-style-type: none"> <li>• Very tidy and 'easy-care' but will require edging of grass/concrete margins as part of routine reserves maintenance contract work.</li> <li>• Will have a visual impact compared to uninterrupted grass but, with 50% black-oxide colouring it will be suitably presentable and will be substantially more appealing than aggregate. In this investigation report care has been taken to sensitive positioning of the pavement to retain uninterrupted visual open-space as much as possible.</li> </ul>

N.B. Use of asphalt and chip-seal are not considered in this table due to the similarity of cost to concrete pavement but with reduced visual appeal and longevity of service. Also, a wider and more critically prepared sub-base would be required than for concrete pavement.

## 2) Priority Work Sites

If financial, or other, constraints influence the extent of this proposed trail upgrade then initial priority should be given to:

- (i) The work detailed in Appendix III, Sec.2 (being repair of the scoured esplanade reserve margins and remediation of the stop-bank culvert problems from chainage 1,260m to 1,305m). The approx. value of this work is \$88,000.00.
- (ii) Reformation and upgrade of the trail from chainage 112m to 200m. This includes constructing 92m of retaining walls, cutting a new bench-line, infilling & realignment as detailed, concrete finish and upslope batter revegetation). The approx. value of this work is \$46,000.00.
- (iii) Subject to available funds less pressing works can be prioritised accordingly.



**Appendix III. Rough Order of Costs**

**1. Extract from Drafted Excel Spreadsheet for Tenderer Pricing:**

PART 2 - PRICING SCHEDULE - Rates & Estimated Quantities for Yeoman Trail Upgrade, Katikati (excludes culvert site)						DRAFT (for In-house ROC but to be adjusted for tendering purposes)
Park Rd to Chainage 1,205 Plus, 45m at Levley Lane Res. = approx. 1,260 linear metres new concrete pavement						Jan-19
Item No.	Description	Unit	Rate/unit (\$)*	Qty	Total (\$)	Notes to Tenderer
1	Cut-to-fill Earthworks to form 2.7m wide benchline - estimated cut volume over 1,260m length	m <sup>3</sup>		535		Allow for work to be completed over 10 week period with no additional delays due to extreme weather events or archaeology.
2	Earthworks - estimated volume cleanfill sub-base & top soil ground dressing- (delivered & laid, includes costs to prep. finished contours)	m <sup>3</sup>		217		
4	Tree pruning & scrub clean-up	approx 500m <sup>2</sup> scrub; 2xtrees to crown-lift		1		Arborist to crown-lift two specimen trees. Remove banana palm clump from site. Mulch weeds & scrub on batters as required.
5	Silt fencing or alternative sediment management	lin. mtrs		200		Includes supply & place
6	Site safety management	1		1		Includes site-specific safety plan, installation of safety fencing for 10 week period and site safety noticeboard.
7	Retaining Wall - Supply materials & construct walls	1m(h) x 50m(l)		1		See table of construction details
8	Retaining Wall - Supply materials & construct walls	2m(h) x 60m(l)		1		See table of construction details
9	Vibra-compaction roll sub-base at each 0.5m depths of fill and to prep. sub-base finish over full pavement length	1		1,260		Includes supply and placement of approx. 216m <sup>3</sup> clay or GAP40/20 (or similar) as concrete prep and vibra-compaction.
10	Supply & lay 20MPa Concrete Pavement (505m x 2.5m x 0.1m); with 50% black oxide additive	m <sup>2</sup>		3,150		Prepared path boxed
11	Make contraction control joints at 3m separation (1,260/3= 420 joints)	all 420 joints		1		
12	Grass-seed bare ground - supply & spread (estimate 6,000m <sup>2</sup> )	m <sup>2</sup>		6,000		Grass seed to have a light topsoil cover
13	Site finishing & clean-up; includes removal of all debris & greenwaste from site	1		1		
14	Supply & lay one 315mmØ pipe (Farmboss or equiv.) at chainage 186m	1		1		Lay pipe before infilling. (See cross-section 'G' at chainage 186m)
15	Council Signage - Install public information signs at each end of works	1		1		Signs supplied by WBOPDC.
16	Other - personal transport	1		1		
17	Other - heavy machine establishment	1		4		
18	Contingency budget (15%)	1		1		May be required for additional drainage, fill, geotech fabric, earthworks etc.
	<b>TOTAL</b>					

\* All rates are GST exclusive.

**2. This table is copied from Cross-section 'Z' herein. These costs are additional to the above table:**

**VOLUME 8**

From chainage 1,260m to 1,305m (alongside channel scour site to stop-bank). Total distance for concrete pavement = 45m. Total earthworks in this sector extends to replacement of the culvert with a flat-rack bridge and channel-edge armouring & repair.

[This itemised cost tabulation is for staff estimation purposes only.]

Item	Details	\$ (Value) (estimated)
Non-woven Geotextile fabric AS640 or equivalent (e.g. A49 or 1160NP)		
Boulder rip-rap (0.7m to 1.0m dia.)		
Construct rip-rap armouring		
Clean-fill imported & laid		
Sundry allowance		
Culvert Earthworks + removal & disposal of culvert (Machinery & Labour)		
Flat-rack bridge		
Resource Consent matters for works within CMA?		
Pavement sub-base prep. & lay concrete (all-in cost)		
Unforeseen		
	<b>TOTAL</b>	

**3. Archaeological Fees & Cultural Monitoring – Allow \$15,000.00**

**4. Landscape plant supply, site prep. & planting above retaining wall (minor works, by separate contractor) – Allow \$5,000.00**

**5. Fees: – Resource Consent for culvert replacement; Plus, Council project management – Allow \$10,000.00**

**6. Combined Costs** (includes all pavement and replacement of culvert with bridge & maximum scour repairs as detailed, plus various fees).

# ATTACHMENT A

[REPORT ENDS]

(\$352,000 + \$88,000 + \$15,000 + \$5,000 + \$10,000) = **\$470,000.00** (GST exclusive).

**Western Bay of Plenty District Council**  
**Operations & Monitoring Committee**  
**Additional Opening Hours at Te Puke Recycle Centre &**  
**Katikati Recycle Centre**

**Purpose**

To advise Council that both the Te Puke and Katikati Recycling centres have been operating under additional hours in response to requests from both the Community Board and general public.


The associated costs for the extra hours for Te Puke Recycle Centre equates to \$29,952 and Katikati/Athenree \$36,337 per year.

**Recommendation**

- 1. THAT the Resource Recovery and Waste Team Leader's report dated 13 March 2019 titled Additional Opening Hours at Te Puke Recycle Centre & Katikati Recycle Centre be received.**
- 2. THAT the report relates to an issue that is considered to be of low significance in terms of Council's Significance and Engagement Policy.**
- 3. THAT the Operations & Monitoring Committee approve the additional opening hours for the Te Puke Recycle Centre to operate from 8am to 5pm on Tuesdays and Thursdays and the cost implications of an additional \$29,952 per annum be absorbed within the existing operational budget.**

**AND**

- 4. THAT the Operations & Monitoring Committee approve the additional opening hours for the Katikati Recycle Centre to operate from 9am to 4pm on Thursdays and the cost implications of an additional \$36,636 per annum increase for additional staff hours required for the Katikati and Athenree Recycle Centres on Thursdays be absorbed within the existing operational budget.**

  
 Resource Recovery and Waste Team  
 Leader

Approved:



Gary Allis  
 Deputy Chief Executive

## **1. Background – Te Puke Recycle Centre**

### **1.1 Te Puke Community Board's Request for Extended Hours**

In response to community demand, in early 2018 the Te Puke Community Board requested Council to consider extending the opening hours at the Te Puke Recycle Centre. This was to allow the public to drop-off their recyclables earlier in the morning and later in the afternoon. The extended opening hours would allow customers from areas further away than Te Puke, such as Maketu and Paengaroa to recycle on their way to and from home.

Council then applied for a change to the Resource Consent that determined the hours of operation of the centre. A lengthy process followed to amend the Resource Consent to allow for additional operational hours at the centre. The process included consultation with all neighbours, including KiwiRail. The Consent changes were eventually approved and hours of operation were extended.

### **1.2 Change in Kerbside Glass Collection**

In March 2018, Waste Management Ltd stopped collecting glass in all their kerbside collections. This impacted on all private waste collectors in the district as the sorting facility in Te Maunga, owned and operated by Waste Management Ltd., no longer accepted glass for recycling.

Many customers of Waste Management Ltd cancelled or did not renew their contract with Waste Management. The Te Puke Recycle Centre (as Council's other recycle centres) experienced an increase in the amount of glass and other recyclables coming into the centre for recycling. There was a notable increase in people visiting the centres from April 2018.

### **1.3 Trialling Extended Opening Hours in September 2018**

With the Resource Consent amendments for the centre approved, staff started trialling extended opening hours at the Te Puke Recycle Centre in September 2018.

The Centre opened to the public from 8am to 5pm on Tuesdays and Thursdays. Saturday hours remained the same (8am to 4pm). The normal opening hours for Tuesdays and Thursdays are 9am to 3:30pm.

A survey to determine where customers live and how they use the centre accompanied the trial from Tuesday, 4 September to 29 November 2018. Customers coming into the centre were asked five simple questions using an iPad.

A vehicle count was also kept to specifically record the first hour (8am to 9am) and last hour (4pm to 5pm) on each Tuesday and Thursday during the trial period.

**Attachment A**

#### **1.4 Staffing implications for extended opening hours**

To ensure Council complied with employment laws, staffing hours were increased to allow for continuous customer service during opening hours and provide rolling lunch breaks for staff as the centre does not close over lunch time.

Existing permanent, part-time staff were used to cover allowing some staff to start earlier and others to finish later. Where this could not be achieved, the hours were covered by using a casual contracted staff member or staff provided by Tradestaff.

Staffing the centre requires a minimum of three staff at any one time. One staff is on the fork hoist changing bins, another works both inside and outside the shed attending to customers and another deals with customers paying for greenwaste drop-off.

Staff at the Te Puke Recycling Centre are employed on permanent part-time contracts with specified hours that cover servicing the public during opening hours and providing for hours to undertake the baling of product of recyclables such as cardboard and plastics. Baling product and loading out to trucks are done on days that the centre is closed to the public - usually on a Wednesday or Friday.

If the extended opening hours are formalised, a need to approve additional staffing hours is needed. The financial implication to provide enough staffing hours is covered below.

#### **1.5 Decision to Continue with Additional Hours**

At the end of November 2018, an operational decision was made to continue with the extended hours for the busy holiday period of December 2018 and January 2019. Customers at the centre have become accustomed to the extended hours. We believe that going back to the previous hours would create dissatisfaction for Council, especially with the continued inability to recycle glass from the kerbside.

A formal Council decision is now sought to formalise the extended opening hours with the increase in the level of service (LOS) at the Te Puke Recycle Centre. The increase in LOS has a financial impact as more staffing hours are needed to serve the public and to process the increased volumes of recyclables coming into the centre.

Council's waste minimisation vision is to "Minimise Waste to Landfill". The continued operation of the Te Puke Recycle Centre, until other rubbish and recycling models are decided on by Council, works towards reaching this vision.

#### **1.6 Staffing cost to continue with the increased level of service**

To enable the Te Puke Recycle Centre to operate from 8am to 5pm on Tuesdays and Thursdays will require an additional \$29,952 pa.

## **2. Background – Katikati Recycle Centre**

Katikati Recycle Centre has shown a steady increase in new and existing customers. This increase in customer numbers resulted trialling extended opening hours at the centre on a Thursday. This resulted in improvements at the centre including better traffic management and also how staff operate the centre. Reasons for the extending the opening hours is discussed below.

**2.2 No Kerbside Collection of Glass for Recycling**

In March 2018, Waste Management Ltd decided to stop collecting glass in all their kerbside collections. This impacted on all private waste collectors in the district as the materials sorting facility in Te Maunga, owned and operated by Waste Management Ltd., no longer accepted glass for recycling.

Many customers of Waste Management Ltd. cancelled, or did not renew their contact with Waste Management at this point. The Katikati Recycle Centre (as Council's other recycle centres) experienced an increase in the amount of glass and other recyclables coming into the centre for recycling.

With nowhere to recycle glass in Katikati, vehicle movement through the centre increased significantly and staff struggled to provide the recognised level of service to customers. Staff also struggled to maintain the required health and safety standards for operating the centre under the normal opening hours.

**2.3 Requirement to open both Katikati and Athenree Recycle Centres on the same day with the same staff**

The normal opening hours for Council's western recycle centres at Katikati and Athenree are as follows:

***Athenree: 64 Steele Road, Athenree***

Thursday: 1pm - 4pm  
Saturday: 9am - 12noon  
Sunday: 1pm - 4pm

***Katikati: Corner Tetley/Wills Road***

Thursday: 9am - 12noon  
Saturday: 9am - 4pm  
Sunday: 9am - 12pm

The normal opening hours require staff to close the Katikati Recycle Centre at 12noon Thursdays. In the hour before 1pm they need to eat lunch, travel to Athenree and then be ready to open the gates at 1pm for the Waihi Beach and Athenree customers.

With the sheer volume of customers going through the Katikati Recycle Centre on Thursday mornings, staff struggled to cope with servicing customers in a timely manner and also manage traffic congestion at the centre. When the gates close at 12noon it took staff up to 30 minutes to clear the centre at Katikati before they had to rush to open up at Athenree.

**2.4 Opening Katikati Recycle Centre for a full day Thursdays**

To provide a solution to traffic congestion, Council conducted a trial during the month of May 2018 on each Thursday by extending the opening hours of the recycling centre from 9.00am to 4.00pm. (The normal opening hours were from 9.00am to 12 noon.)

On these days (Thursdays) a short survey was conducted to determine the recycling habits of the Katikati community. Customers were asked questions using Survey Monkey on an iPad. We wanted to know if our Katikati customers would change their habits and use the centre on Thursday afternoons.

If customers were prepared to change their drop-off to Thursday afternoons, the rush of vehicles going through the centre on Thursday mornings could be alleviated.

## **2.5 Survey Results and Vehicle Numbers**

Staff analysed the survey results from customers and agreed that there was enough desire by customers to change their drop-off behaviour to utilise the centre on Thursday afternoons. Towards the end of May 2018, it was noted that vehicle numbers started to decrease on Thursday mornings and a steady vehicle flow observed during the whole day that the centre was open.

Prior to May 2018, an average of 355 vehicles were processed at the Katikati recycle centre in a three hour period. As part of the trial a vehicle tally was kept to record the change in behavioural patterns of customers using the site. The numbers of vehicles during the trial from 9am to 12noon averaged 230 and from 12noon to 4pm averaged 125 vehicles, hence on average 355 vehicles Thursdays. This makes for a much better experience in trying to find a park to unload recyclables.

## **2.6 Happy customers – Safer Experience**

Feedback from recycle centre staff and customers have been overwhelmingly positive. The reasons given are:

- less vehicles;
- more available parking spaces;
- customers not rushed to complete their recycling;
- fewer near miss incidents involving vehicles and angry customers; and
- Improved health and safety onsite.

In addition, staff are now free to actually assist customers with their recycling and are not constantly marshalling traffic through the centre.

The trial has continued throughout the winter and summer periods and the vehicle numbers have not reverted back to old patterns as customers have become accustomed to the change.

## **2.7 Staffing level requirements**

To enable the Katikati Recycle Centre to stay open for a full day each Thursday, (9am to 4pm) the current permanent part-time staff are rostered in such a way that both the Katikati and Athenree Recycle Centres (open from 1pm to 4pm on Thursdays) are adequately staffed. We also use staff from Tradestaff when required to ensure adequate staffing levels at both centres.

## **2.8 Additional costs associated with Katikati Recycle Centre opening longer are \$36,636.96 based on current labour rates.**

Current budget is tracking well under for this financial year and these costs of \$36,636.96 have been accounted for in the budget.

### 3. Significance and Engagement

In terms of the Significance and Engagement Policy these decisions are considered to be of low significance as the affected parties requested the increase in hours increased level of service. The decisions are reversible should the recycling environment require it.

### 4. Engagement, Consultation and Communication

Engagement with the community (users of the centres) has been positive and a higher expectation is that the centres will continue opening under these extended hours. It is anticipated that if Council adopts the new opening hours, a general communication will be sent to all rate payers within the areas of benefit.

Interested/Affected Parties	Completed/Planned Engagement/Consultation/Communication
Name of interested parties/groups	Te Puke: Completed with the Te Puke Community Board, Te Puke Baptist Church and neighbours around the Te Puke Recycle centre including Kiwi Rail.  Katikati: Katikati Community Board and neighbours of the Katikati Recycle Centre.
Tangata Whenua	Not specifically consulted
General Public	Survey at centres, newspaper and social media communications

### 4. Issues and Options Assessment

<i>Option A</i>
<p><b>3. THAT the Operations &amp; Monitoring Committee approve the additional opening hours for the Te Puke Recycle Centre to operate from 8am to 5pm on Tuesdays and Thursdays and the cost implications of an additional \$29,952 per annum be absorbed within the existing operational budget.</b></p> <p><b>AND</b></p> <p><b>4. THAT the Operations &amp; Monitoring Committee approve the additional opening hours for the Katikati Recycle Centre to operate from 9am to 4pm on Thursdays and the cost implications of an additional \$36,636 per annum increase for additional staff hours required for the Katikati and Athenree Recycle Centres on Thursdays be absorbed within the existing operational budget.</b></p>



<p><b>Assessment of option for advantages and disadvantages taking a sustainable approach</b></p>	<p>Confirm the extended hours of operation for the Te Puke and Katikati Recycle centres and approve the cost implications of the increase in level of service for both centres, as per the report.</p> <p>The increase in LOS at the centres has been requested by the public. The Te Puke Community Board who requested the extension of the hours on behalf of the public can report to their community that Council took their request seriously.</p> <p>The centres provide an opportunity for glass to be collected that may have gone to landfill since kerbside services ceased due to a decision by the private sector.</p> <p>Also, with increase in patronage from customers to the centres has resulted in more product being recycled – this outcome minimises waste to landfill and has positive environmental and financial outcomes.</p> <p>The increase in LOS has already improved health and safety for customers at the centres, especially Katikati Recycle Centre.</p>
<p><b>Costs (including present and future costs, direct, indirect and contingent costs) and cost effectiveness for households and businesses</b></p>	<p>Te Puke: Wage cost of \$29,952 per annum for additional staffing hours at the centre.</p> <p>Katikati: Wage cost of \$36,636.96 per annum for additional staff hours at the centre.</p>
<p><b>Other implications</b></p>	<p>The decision can be reversed should it be required by policy changes or future Council decisions on rubbish and recycling models.</p>

## 5. Statutory Compliance

The recommendation meets:

- Legislative requirements/legal requirements
- Current council plans/policies/bylaws
- Regional/national policies/plans.

## 6. Funding/Budget Implications

Budget Funding Information	Relevant Detail
<p><b>To be funded through the existing operational budgets for the centres</b></p>	<p>Te Puke: Wage cost of \$29,952 per annum Katikati: Wage cost of \$36,636 per annum</p>

## Te Puke Recycle Centre – Survey Results

### Results of the Survey Questions and Vehicle Count

A total of 734 customers were surveyed over a six week trial period on Tuesdays and Thursdays.

#### Question 1 – What area do you live in?

Te Puke Town	60%
Te Puke Rural	20%
Paengaroa	10%
Maketu	6%
Other	3%

#### Question 2 - How often do you use the Te Puke Recycling Centre?

Once per week	37%
Twice per week	5%
Three times per week	3%
Once per month	26%
Twice per month	26%
Three times per month	2%
Other	1%

#### Question 3 - Which day do you usually visit the Te Puke Recycling Centre?

Tuesday	30%
Thursday	13%
Saturday	7%
All of the above	50%

#### Question 4 - If we open the centre from 8:00am to 5:00pm on Tuesdays and Thursdays, will you visit the centre between 8:00am – 9:00am and/or 3:30pm – 5:00pm?

YES	80%
NO	20%

Comments from those that answered NO include: retired, mother with young children; work done at home before I come; Require a later open time like 6:30pm.

#### Question 5 – Asked for contact details

405 respondents provided contact details  
329 respondents did not provide contact details

#### Vehicle numbers during the first and last hour of the trial

Vehicle numbers from 8am to 9am averaged 32 vehicles in the first opening hour and 12 vehicles in the last hour from 4pm to 5pm. The total amount of vehicles visiting the centre on Tuesdays and Thursdays averaged 350 vehicles.

## Western Bay of Plenty District Council

### Infrastructure Committee

# Omokoroa Golf Course and Precious Family Reserve Walkway Erosion Funding Report

## Purpose

To formalise the Council's commitment to the joint project between the Omokoroa Golf Club and Council to consent and construct a bund, culvert structures and walk/cycleway on the esplanade reserve that bounds the golf course.

To note the potential funding sources.

To note that the project is linked to the filling of Precious Reserve.

## Recommendation

1. ***THAT the Specialist Engineers report dated 12 March 2019 and titled Omokoroa Golf Course and Precious Family Reserve Walkway Erosion Funding Report be received.***
2. ***THAT the report relates to an issue that is considered to be of medium significance in terms of Council's Significance and Engagement Policy.***
3. ***THAT Council agrees to a joint project between the Omokoroa Golf Club and Council to consent and construct a bund, culvert structures and a walk/cycleway on the esplanade reserve that bounds the golf course and that consents, fills and re-contours the lower section of Precious Reserve.***
4. ***THAT it be noted that funding for the project may include:***
  - ***Omokoroa Golf Club contribution (confirmed) \$250,000***
  - ***Omokoroa Community Roading allocation \$200,000 (subject to Community Board decision)***
  - ***Relocation of surplus material from the Omokoroa Pond 2 construction indicative \$50,000***
  - ***Omokoroa Structure Plan walkway allocation up to \$200,000***
  - ***External funding Balance***

Date 12 March 2019

**Open Session**

Subject: Omokoroa Golf Course and Precious Family Reserve Walkway Erosion Funding Report

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Peter Clark

**Specialist Engineer**

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Approved



Gary Allis

**Deputy Chief Executive**

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## 1. Background

The properties at both Precious Family Reserve and the Golf Course including the esplanade walkway esplanade have been the subject of frequent inundation and erosion from the sea. The properties are located as shown below.



The Golf Club has sought assistance for many years to mitigate on going erosion, salt water flooding, grass burn, and sand deposition. Council has a partially defined walkway from Precious Family Reserve to Kayelene Place around the perimeter of the course. However unless works are undertaken to protect the walkway, this will ultimately be lost to the sea. The walkway is a key component of the Omokoroa walk/cycleway network and if fully developed will be a popular recreational route.

The walkway is a key component of the Omokoroa Walkway/Cycleway network and if fully developed will be a popular recreational route.

Council has responded with various reports on potential solutions provided by consultants, while ad hoc solutions have been provided by club members.



*Typical ad hoc bridge solution:*



*Typical erosion and retreat is shown in the photograph above.*

Inundation is shown below: (Photos by Murray Grainger January 2018)



*View south from Precious Family Reserve toward the golf club.*



*The seat and shelter (with swimmer) at Precious Family Reserve.*

Typically damage consists of erosion, burned grass with deposited sand and debris.



During events, both the golf course and the walkway are unusable, until the tide recedes and club members typically repair the damage.

However it can be observed that slightly higher areas of land remain undamaged.

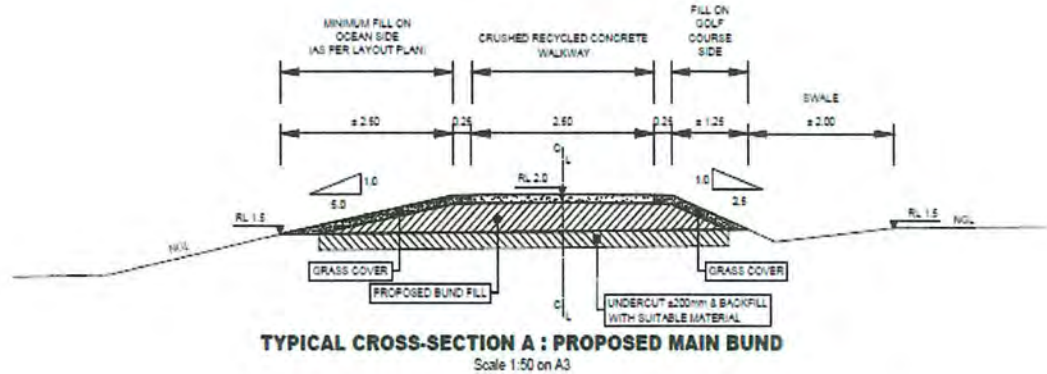


From this observation, an improvement to the situation has been developed. A bund at approximately the height of the high land is proposed to mitigate these events. A walkway would be constructed along the top of the bund, and storm water outlet infrastructure would be installed.





*Approximate location of bund and walkway. Red rings denote storm water outlets.*



Scheme plans and reports have now been developed in support of resource consents from both Regional and District Councils. A consent from Western Bay of Plenty District Council has been approved. The Regional Council consent is being processed.

Surplus earth has been stockpiled at the golf course ready for construction. More is still required, however until consent is issued, no more material can be stockpiled in this sensitive area.

There is an expectation that surplus fill from the Omokoroa Developments could be acquired free or at low cost to use on the bund and the filling of Precious Reserve.

The bund will extend in front of the Precious Family Reserve, which will also be filled to a higher level to prevent water logging. The reserve will then become a useful amenity to the people of Omokoroa during wet periods.

The intention is that the nearby stormwater ponds will be cleaned and silt removed in parallel with the project and the silt will be mixed into the Precious Reserve filling. This is a cost effective option.

A rough order cost estimate to complete the works has been provided by a contractor and approximates \$1.1 million dollars. Significant contingency sums are

included in this estimate and it is not intended to purchase fill/bund material, rather wait for the material to be delivered as surplus to nearby developers needs.

A more refined estimate will be developed once final design has been undertaken on completion of the consenting phase.

However, the purpose of this report is to assess the funding needs for the work and identify likely sources. These are identified in Section 6 Funding/Budget, however are repeated here

- |  |           |
|--|-----------|
| • Golf Club  | \$250,000 |
| • Community Roding allocation                                      | \$200,000 |
| • Relocation of material from Pond 2<br>(instead of cart to waste) | \$50,000  |
| • Structure Plan walkway budget up to                              | \$200,000 |

Additional funding may be available to the Golf Club from:

- NZ Community Trust - sports
- Lottery Community Facilities Committee – Bay of Plenty/Gisborne
- Lottery Community Committee National
- Lottery Outdoor Safety
- Pub Charity
- NZ Racing Board Sport Grant
- The Southern Trust
- BayTrust
- Department of Internal Affairs
- First Sovereign Trust NZ
- Grass Roots Trust (gaming)
- Lion Foundation
- Oxford Charitable Sports Trust
- TECT
- The Trust Charitable Foundation Inc.

At this stage, the intention is to work with the Golf Club and achieve a result that:

- protects the golf course from most storm events; and
- creates a clearly defined and formed walk/cycleway link

The Council funding is targeted towards the bund and trail connection.

## 2. Significance and Engagement

The Local Government Act 2002 requires a formal assessment of the significance of matters and decisions in this report against Council's Significance and Engagement Policy. In making this formal assessment there is no intention to assess the importance of this item to individuals, groups, or agencies within the community and it is acknowledged that all reports have a high degree of importance to those affected by Council decisions.

The Policy requires Council and its communities to identify the degree of significance attached to particular issues, proposals, assets, decisions, and activities.

In terms of the Significance and Engagement Policy this decision is considered to be of low significance as funding from significance because while Council is being asked to contribute, and will benefit from an improved walkway and reserve, much of the funding will be from the community.

### 3. Engagement, Consultation and Communication

Interested/Affected Parties	Completed/Planned Engagement/Consultation/Communication
Name of interested parties/groups	Omokoroa Golf Club Omokoroa Community Board Bay of Plenty Regional Council: Consent Required – in process Western Bay of Plenty District Council: Consent required: granted
Tangata Whenua	Consultation with Pirirakau representatives and other representatives as instructed by Regional Council has been undertaken as part of the Consent process
General Public	Walkway is identified in the Reserves management plans

#### 4. Issues and Options Assessment

<b>Option A</b>	
<p><b>3 THAT Council agrees to a joint project between the Omokoroa Golf Club and Council to consent and construct a bund, culvert structures and a walk/cycleway on the esplanade reserve that bounds the golf course and that consents, fills and re-contours the lower section of Precious Reserve.</b></p> <p><b>4. THAT it be noted that funding for the project may include:</b></p> <ul style="list-style-type: none"> <li>• <b>Omokoroa Golf Club contribution (confirmed) \$250,000</b></li> <li>• <b>Omokoroa Community Roading allocation (subject to Community Board decision) \$200,000</b></li> <li>• <b>Relocation of surplus material from the Omokoroa Pond 2 construction indicative \$50,000</b></li> <li>• <b>Omokoroa Structure Plan walkway allocation up to \$200,000</b></li> <li>• <b>External funding Balance</b></li> </ul>	
<p><b>Assessment of option for advantages and disadvantages taking a sustainable approach</b></p>	<p>With the joint project and indicative funding arrangement the works will proceed.</p> <p>Advantages:</p> <ul style="list-style-type: none"> <li>• Golf Course will remain viable for many years to come. (Estimate at least 50 years based on 30cm sea level rise)</li> <li>• The walkway will be a key amenity to the community</li> <li>• Precious Family reserve will be useful as a recreational reserve and not become water logged as now.</li> <li>• The Golf Club will provide a level of free support to the Council walkway for a protective bund that they have requested over many years.</li> <li>• New outlet structures will be formed as part of the works improving the safety of walkers on the walkway.</li> </ul> <p>Disadvantages</p> <ul style="list-style-type: none"> <li>• Erosion monitoring will be required to ensure that the bund will be repaired after major events.</li> <li>• There will be an on going cost of maintenance for the walkway and planting.</li> </ul>
<p><b>Costs (including present and future costs, direct, indirect and contingent costs) and cost effectiveness for households and businesses</b></p>	<p>Total works costs will be of the order of \$1.1million, and this will be shared with the community.</p> <p>Actual cost will depend on consent, final design and availability of fill material.</p> <p>Ongoing maintenance costs will be shared, with the expectation that the Golf Course will provide the mowing, and likely Council the remaining maintenance.</p>
<p><b>Other implications</b></p>	<p>An initial cost share has been agreed in order to begin consultant works, however capital works are likely to proceed only as funding is made available. Final cost estimates will only be finalised once final design is complete.</p> <p>Additional funds need to be found from benevolent trusts in order to progress these works</p>

<b>Option B Status Quo</b>	
<b><i>THAT Council does not proceed with a joint project with the Golf Club.</i></b>	
<b>Assessment of option for advantages and disadvantages taking a sustainable approach</b>	<p>Once consents are in place, the Golf Club may try to fund part of the bund, but are unlikely to be able to complete the works. No works would proceed at Precious Family Reserve.</p> <p>With more frequent inundation from the sea expected, the existing walkway would slowly deteriorate to be useful at low tide only, and use of the golf course may become difficult. The advantage to Council of doing nothing is that nature takes its course and the onus on fund raising remains with the owners of the golf course. However, it is expected that the Club will endeavour to complete some of the works on their own, but without new storm water structures at each drain outlet (Council infrastructure), the works would be ineffective.</p> <p>If Council did not contribute</p> <p>Advantages:</p> <ul style="list-style-type: none"> <li>• Nature takes its course</li> <li>• No cost to Council</li> <li>• No maintenance costs</li> </ul> <p>Disadvantages</p> <ul style="list-style-type: none"> <li>• Loss of amenity at Precious Family Reserve</li> <li>• Gradual loss of amenity at the Golf Club</li> <li>• Continuing requests from Golf Club and community</li> <li>• Loss of coast line through erosion</li> </ul> <p>The key walkway connection would not be constructed and enjoyed by the community.</p> <p>No significant works would then take place, without Council assisted funding.</p>
<b>Costs (including present and future costs, direct, indirect and contingent costs) and cost effectiveness for households and businesses</b>	No additional costs will be incurred by Council if the works do not proceed.

## 5. Statutory Compliance

The recommendation provides direction to Staff and the community by the elected members

## 6. Funding/Budget Implications

Budget Funding Information	Relevant Detail										
<p>Funding is likely to be found from various organisations.</p> <p>An initial budget of \$1.1m is indicated</p>	<p>While this project is not budgeted in the current financial year, a submission will be made to the annual plan, confirming the Club offer and reflecting the following request for Council funds, either in the 2019/2020 year or in subsequent years:</p> <table data-bbox="671 611 1410 1081"> <tr> <td data-bbox="671 611 1241 689">Omokoroa Golf Club contribution (confirmed)</td> <td data-bbox="1241 611 1410 689">\$250,000</td> </tr> <tr> <td data-bbox="671 696 1241 801">Omokoroa Community Roothing allocation (subject to Community Board decision)</td> <td data-bbox="1241 696 1410 801">\$200,000</td> </tr> <tr> <td data-bbox="671 808 1241 936">Relocation of surplus material from the Omokoroa Pond 2 construction indicative</td> <td data-bbox="1241 808 1410 936">\$50,000</td> </tr> <tr> <td data-bbox="671 943 1241 1021">Omokoroa Structure Plan walkway allocation</td> <td data-bbox="1241 943 1410 1021">up to \$200,000</td> </tr> <tr> <td data-bbox="671 1028 1241 1081">External funding</td> <td data-bbox="1241 1028 1410 1081">Balance</td> </tr> </table>	Omokoroa Golf Club contribution (confirmed)	\$250,000	Omokoroa Community Roothing allocation (subject to Community Board decision)	\$200,000	Relocation of surplus material from the Omokoroa Pond 2 construction indicative	\$50,000	Omokoroa Structure Plan walkway allocation	up to \$200,000	External funding	Balance
Omokoroa Golf Club contribution (confirmed)	\$250,000										
Omokoroa Community Roothing allocation (subject to Community Board decision)	\$200,000										
Relocation of surplus material from the Omokoroa Pond 2 construction indicative	\$50,000										
Omokoroa Structure Plan walkway allocation	up to \$200,000										
External funding	Balance										
Total Required from all sources	\$1.1M										

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## Western Bay of Plenty District Council

### Operations Committee

## Housing Action Plan Six-monthly Progress Report

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### Purpose

Council adopted the Housing Action Plan in October 2018, with a requirement to provide six-monthly progress reports to the Operations Committee.

This report presents the first progress report.

**Attachment A**

### Recommendation

- 1. THAT the Senior Policy Analyst's report dated 19 March 2019 and titled Housing Action Plan Six-Monthly Progress Report be received.***
- 2. THAT the report relates to an issue that is considered to be of low significance in terms of Council's Significance and Engagement Policy.***



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Jodie Rickard  
**Senior Policy Analyst**

## 1. Background

Council adopted the Housing Action Plan in October 2018. The Action Plan is largely Council's response to the Housing Demand and Need Assessment report which was finalised in December 2017.

The Housing Action Plan brings together the work Council is doing to contribute to the overall district vision – that all Western Bay residents are well-housed. There are four focus areas:

Housing that is:

- Affordable
- Accessible
- Habitable
- With security of tenure.

The Housing Action Plan aligns with SmartGrowth's Smart Housing Action Framework.

### 1.1 Progress Report

The progress report is attached as Appendix A. It includes background information for each action, progress over the last six months, and next steps.

### 1.2 Highlights

Strategic land purchase in Katikati – Council purchased 1.657 hectares next to Moore Park in Katikati, for extension of the park and possible housing developments.

Workshop run by BayTrust on establishing a BOP Housing Impact Fund, to support development of assisted rental or assisted ownership housing.

**A Healthy Whare Project:** - Continuing to work to deliver healthy housing, in Katikati. The project team is also talking to other communities with housing needs.

**Bay of Plenty Healthy Homes Forum work:** - The Forum is developing a regional healthy homes programme, that will be able to leverage central government funding (in particular from EECA) to improve housing quality.

### 1.3 Targets

#### Affordable Housing

**Target 1:** 110 additional affordable housing units are delivered across the district over the next 3 years.

Council is working with other providers on opportunities in Katikati to develop assisted ownership or assisted rental models.

The development of the affordable housing block in the Omokoroa SHA is also progressing, with the land value now set by Council.



**Target 2:** Deliver a new Maori Housing Strategy to be in place by 2019 (this will include its own targets).

Progress: This action has not yet been progressed. However some research has been done with Maori Land Trusts which will inform this work.

### **Habitable Housing**

**Target 3:** 100 homes are warmer, drier and safer over the next 3 years.

Through A Healthy Whare Project 37 homes have been assessed and have had improvements made so they are warmer, drier and safer.

Te Puna have included A Healthy Whare Project in their community plan. This will be the next area for consideration.

### **Accessible Housing**

**Target 4:** A Plan change for the residential zone has been notified.

This work is included in the resource management work programme for 2019

**Target 5:** Changes have been made to the Infrastructure Development Code to deliver more accessible neighbourhoods.

This work has not yet progressed, but will align with the District Plan changes to the residential zone.

**Target 6:** Methods to deliver more accessible homes (including incentives for meeting certain accessibility standards such as Lifemark accreditation) are explored, with recommendations being implemented.

This work has not yet commenced.

## **2. Significance and Engagement**

The Local Government Act 2002 requires a formal assessment of the significance of matters and decisions in this report against Council's Significance and Engagement Policy. In making this formal assessment there is no intention to assess the importance of this item to individuals, groups, or agencies within the community and it is acknowledged that all reports have a high degree of importance to those affected by Council decisions.

The Policy requires Council and its communities to identify the degree of significance attached to particular issues, proposals, assets, decisions, and activities.

In terms of the Significance and Engagement Policy this report is considered to be of low significance because it is a report for information only. No decision is required.

### 3. Engagement, Consultation and Communication

Interested/Affected Parties	Completed/Planned Engagement/Consultation/Communication
SmartGrowth	The Housing Action Plan aligns with the SmartGrowth Smart Housing Action Framework. The SmartGrowth leadership group receive progress reports on the Smart Housing Action Framework – this progress report will inform those reports.

### 4. Issues and Options

Insert précis of resolution required	
Reasons why no options are available	Legislative reference or other
This report is for information only. There is a recommendation to receive the report.	

### 5. Statutory Compliance

This report delivers on the resolution of the Policy Committee on 18 October 2018:

*THAT the Policy Committee directs staff to provide updates on the Housing Action Plan implementation at six-monthly intervals.*

### 6. Funding/Budget Implications

This report does not have any additional funding implications. The current budget is set out below.

Budget Funding Information	Relevant Detail
<b>Project 341001 – A Healthy Whare Project.</b> \$40,000 per annum.	Budget approved through Long Term Plan 2018-2028.
<b>Project 339201 – Review pensioner housing</b> \$30,000.	Budget approved through Long Term Plan 2018-2028.
<b>Project 341002 – Implementation of the Housing needs assessment</b> \$20,000 per annum.	Budget approved through Long Term Plan 2018-2028.
<b>Project 323201 – Papakainga Development</b> \$55,000 per annum.	Budget approved through Long Term Plan 2018-2028.

## Progress Report

### AFFORDABLE HOUSING

<b>Q1: Emergency and Social Housing</b>				
	<b>Actions</b>	<b>Background</b>	<b>Update</b>	<b>Next Steps &amp; Recommendations</b>
1.	Remain an active member of Our Community Project and contribute to research undertaken by that group.	This group facilitated the establishment of The Peoples Project in Tauranga, and has carried out research into addressing homelessness (in particular chronic homelessness – people living on the streets).	Our Community Project has been disestablished. The group was facilitated by TCC with Steph O'Sullivan (formerly of Ngati Ranginui Iwi) as the Chair. Steph has moved on from Tauranga and TCC have decided not to continue facilitating this group.  The staff views is there is still a need to continue work on homelessness, in particular with women and children who are often homeless, but not living on the streets (couch surfing, in campgrounds etc.).	There may be other groups that can continue to progress research and action for homelessness, including the Tauranga Social Housing Advisory Group (facilitated by Accessible Properties).  However at this stage staff recommend resources be focussed on Te Puke as the priority – see more in action 2 below.
2.	Investigate potential social housing developments in Te Puke, in partnership with other potential providers.	There are several housing providers in Te Puke. The purpose of this work is to better understand what the social housing needs are, and how they could best be met.	A network of providers has been established in Te Puke under the name Te Puke Co-lab. One of the workstreams of this group is housing. Staff have been working with this network to progress this action around social housing in Te Puke. Local research into housing needs is being progressed. The group is planning a housing hui in mid-2019 to discuss the research and to work together on actions.	Staff will continue to work through Te Puke Co-lab, on: - Local research on housing needs specific to the Te Puke community. - A housing hui, to be held in mid 2019.
3.	Support The People's Project Housing First initiative in Tauranga, in particular work to build capacity for other groups to deliver housing first initiatives.	The People's Project Tauranga opened in June 2018. The project is working exclusively with 'rough sleepers', to firstly house them, and then provide support to keep them housed. The People's Project Tauranga is being managed by the WISE Group, under a contract with Ministry of Social Development. WBOPDC are represented on the steering group for The People's Project.	To date, 22 people have been housed by The People's Project. Finding accommodation is a big challenge, as there is a shortage of both private rentals and social housing.	WBOPDC will continue to be represented on the steering group.
<b>Q2 and Q3: Assisted Rental and Assisted Ownership</b>				
	<b>Actions</b>	<b>Background</b>	<b>Update</b>	<b>Next Steps &amp; Recommendations</b>
1.	Continue to provide pensioner housing – review the activity with a view to supporting the delivery of more pensioner housing.	Council currently provides 70 pensioner housing units. The Housing Demand and Needs Assessment projected an increased demand for housing for older people.	Scoping paper presented to Council in November 2018.	Staff are progressing a project plan for the section 17A review.
2.	Advocate to central government for the extension of the Income Related Rent Subsidy (IRRS) to Council-owned elder housing units.	Local authorities are currently excluded from receiving the IRRS, which pays providers a 'top up' to market rent to enable tenant's rent to be capped at 25% of their income. If Council could access the IRRS, it could mean a significant increase to the revenue received from pensioner housing.	There is currently no official indication that the government will extend the IRRS to local authorities.	Continue to advocate for the extension of the IRRS to local authorities.
3.	Continue to support the Joint Agency Group work to develop papakainga.	The Joint Agency Group previously funded a coordinator to assist Maori land trusts to work through the Papakainga toolkit, to facilitate development of housing on Maori land. The JAG also promotes other work such as district plan provisions and policies that enable housing to be developed on Maori land.	Council has budgeted \$40,000 to support the work of JAG in 2018/19.  A stocktake of the Trusts that have previously been through the toolkit project is being undertaken, along with research into Maori housing needs.	Update JAG strategic plan.

4.	Partner to carry out research into Maori housing need, and support development of a Maori Housing Strategy.	The Housing Demand and Needs Assessment completed in 2017 identified the need for further research into Maori housing needs. Previous work on the potential of Maori land blocks to be developed informed the development of JAG and its strategic plan	Staff are working through JAG to determine the scope of research required.	Commission research and use this to inform the JAG strategic plan.
5.	Continue to support the process to develop affordable housing on a designated block within the Omokoroa Special Housing Area	A block of land was set aside within the Omokoroa Special Housing Area to progress a specific affordable housing development. Council has an agreement with the Housing Affordability Forum (HAF) to develop this block as a 'pilot project' for affordable housing.	The land remains under agreement with HAF. Council has agreed a market value for the site.	Continue to work with HAF and developers to progress development of this site.
6.	Use Council-owned land to develop joint initiatives for housing developments that contribute assisted rental and assisted ownership models to the market.	Staff continue to explore options to purchase that will meet the HAP and strategic requirements of Council.	Council is continuing to progress options to develop Council-owned land for assisted rental and assisted ownership models. This work is currently focussed on two Council-owned sites in Katikati.	Continue to work with property owners and developers to develop opportunities for affordable housing.
7.	Consider opportunities to purchase land to facilitate the development of housing developments that contribute assisted rental and assisted ownership models to the market (within Council's fiscal parameters).	Staff continue to explore options to purchase that will meet the HAP and strategic requirements of Council.	The purchase of land adjacent to Moore Park in Katikati provides the most recent example of a strategic land purchase with potential for housing development.	Continue to work with property owners and developers to explore opportunities for affordable housing.

## ACCESSIBLE HOUSING

	Key Actions	Background	Update	Next Steps & Recommendations
1.	Review the Post Harvest zone in the District Plan to ensure its fit for purpose to provide for <i>seasonal worker accommodation</i> .	A review of the Post Harvest Zone in the District Plan was approved by elected members, to ensure the zone is fit for purpose for seasonal worker accommodation. Specifically, the stall were to consider provision for on-site seasonal worker accommodation above the current permitted maximum of 75 people.	<p>It is noted that to-date, there has been a limited uptake in the Permitted Activity limit of 75 on-site beds in the Post Harvest Zones for seasonal horticultural workers, and only some Post Harvest Zones have the capacity space-wise to accommodate large accommodation facilities.</p> <p>The industry recognises that to attract, manage and retain seasonal workers for the peak periods they need to provide cost-effective, safe and convenient accommodation. Having work-ready employees "captured" on-site for the relatively short peak harvest period is an advantage.</p> <p>With the forecast growth in kiwifruit production and consequent demand for seasonal workers throughout much of the year, but critically within the harvest period, a more realistic figure of at least 200 on-site beds has been suggested. This is significantly more than the current limit, however proportionally it is reasonably consistent with the projected growth in kiwifruit production to 2030.</p> <p>Council has agreed to consider increasing the permitted limit provided the effects on the environment can be avoided, remedied or mitigated. Advantages and disadvantages have been considered in an Issues and Options paper recently prepared for engagement with the post harvest industry, and discussion through ongoing workshops with Councillors.</p> <p>Advantages include that provision of specific seasonal worker accommodation facilities on-site at the post harvest facility sites should mean that the residential housing stock within the nearby communities will be more readily available for permanent residents. A significant adverse effect of the current approach to accommodation is the impact that housing workers in residential dwellings, has on the already squeezed rental market.</p> <p>Increasing the Permitted Activity limit in Post Harvest Zones has the potential to be a very effective way of the post harvest industry being able to guarantee accommodation for many temporary seasonal workers during peak work times, but also increasingly throughout the year as production increases and on-orchard tasks stretch over a longer time period.</p>	<p>Included within resource management work programme.</p> <p>Gain Council approval to notify a Plan Change (in July 2019) after additional engagement with the post harvest, and workshops with Councillors.</p>
1a.	Review the provisions that cater for the accommodation of seasonal workers <u>generally</u> in the District Plan.	<p>Kiwifruit production is increasing, post harvest facilities are getting larger, and there's an increasing need for large numbers of seasonal workers particularly during the peak harvesting period.</p> <p>This expected growth, additional jobs and consequent demand for accommodation for both temporary and permanent staff has significant flow-on effects for the industry and consequently for Council.</p>	<p>WBOPDC can do their part to assist the horticultural industry to thrive while navigating the challenges posed by rapid growth by making sure that there are no unnecessary barriers to the industry continuing to do their business as usual, and specifically by ensuring that the District Plan's accommodation provisions assist the industry to provide a variety of acceptable and affordable accommodation options for seasonal workers.</p>	<p>Included within resource management work programme.</p> <p>Gain Council approval to notify a Plan Change (in July 2019) after additional engagement with the post harvest, and workshops with Councillors.</p>

		<p>The post harvest industry has identified a shortage of accommodation facilities for horticultural seasonal workers (which include New Zealanders from outside the Region, RSE workers, and Working Holiday Visas).</p> <p>The accommodation shortage applies to seasonal workers employed in all aspects of the horticultural industry.</p> <p>The absolute peak harvest period is about four weeks in duration, the peak period for picking and packing is about three months, but a variety of work termed "seasonal" is available for about ten to eleven months of the year and includes winter pruning, spraying, thinning, pollination, summer pruning, and other orchard management tasks).</p> <p>It has been acknowledged by Council and the horticultural industry that the review of the Post Harvest Zone provisions alone would probably not deal with the issue of accommodation for seasonal workers. Staff therefore are also reviewing the provisions for <i>accommodation facilities</i> and <i>seasonal worker accommodation outside</i> of the Post Harvest Zone.</p>	<p>Council is considering whether revised or alternative District Plan provisions can be made to assist with resolving the shortage of seasonal worker accommodation within zones other than the Post Harvest Zone (which has been addressed above).</p> <p>Staff have discussed various options while reviewing the accommodation options in an Issues and Options paper recently prepared for engagement with the horticultural industry, and for discussion through ongoing workshops with Councillors. The options include:</p> <ul style="list-style-type: none"> <li>- Making increased provision within Industrial Zones.</li> <li>- Increasing the Permitted Activity limit for accommodation facilities in Residential and Rural Zones.</li> <li>- Providing for temporary on-site camps in the Rural Zone.</li> <li>- Providing accommodation in other (non-Post Harvest Zoned) packhouses.</li> <li>- Providing for seasonal worker accommodation facilities in Rural Zones.</li> </ul>	
2.	Deliver the Future Development Strategy to ensure the supply of residential and business land is identified to provide for the next 30 years of growth.	The Future Development Strategy is a requirement of the National Policy Statement for Urban Development Capacity. The work is being progressed jointly between TCC, WBOPDC, and BOPRC.	A draft strategy was approved and released for consultation. Submissions have closed. A draft was lodged with MfE/MBIE	Finalising the FDS in relation to submissions is put on hold till the outcomes from the transportation project UFTI are known.
3.	Explore district-plan and other tools to incentivise the use of Lifemark accreditation	Tools such as Lifemark provide for universal design of new buildings, which can then support 'ageing in place'.	This work has not yet commenced.	This work will remain on the planning schedule for consideration in the 2020 work programme.
4.	Review district plan and Infrastructure development code (IDC) to deliver 8-80 neighbourhoods  <i>Aligns with Subdivision for People project, components will be delivered through that work programme.</i>	<p>The IDC has the ability to set minimum standards for developments, and is a good place to incorporate concepts such as 8-80 neighbourhoods. This means the streetscape and urban form is designed to be fit for purpose for people of all ages.</p> <p>The District Plan includes provisions for the residential zone, which can also be used to deliver 8-80 neighbourhoods.</p>	Staff are currently reviewing the District Plan provisions for the residential zone, as approved through the resource management team's work programme. This work is also aligned with the Omokoroa structure plan development, and is aimed at getting good urban environments including 8-80 neighbourhoods. The IDC review has just commenced with the new DE Manager.	The project team leading the review of the IDC will incorporate this work into it.

## HABITABLE HOUSING

	Actions	Background	Update	Next Steps & Recommendations
1.	<p>Work with project partners to deliver A Healthy Whare Project to more communities.</p> <p>Target : 2 communities over the next 3 years.</p>	<p>A Healthy Whare Project was established in 2012 to work with communities with poor quality housing, with the aim of improving the housing quality for better health and social outcomes.</p> <p>The project is a partnership between Te Puni Kokiri, Bay of Plenty District Health Board, WBOPDC, Sustainability Options, Tauranga Community Housing Trust and Toi Te Ora Public Health. WBOPDC facilitates the group and provides funding of \$40,000 per annum.</p> <p>The project originally operated in Maketu.</p>	In 2018 the project welcomed Nga Matapuna Oranga onto the project team. Nga Mataapuna Oranga are a Primary Health Organisation for six Maori health providers. They have a current focus on improving the housing stock for the communities they operate in. The organisations that make up A Healthy Whare Project are working to support the work of NMO and their health providers.	Delivery plans are currently being prepared for two of the communities NMO is working with. Funding has been granted from Te Puni Kokiri and whanau ora to support housing improvements, with other project partners including Council also providing support.

2.	Contribute to the work of the Regional Healthy Housing Forum.	The regional healthy housing forum aims to provide coordination and facilitation support and advocacy for healthy housing, across the region.	<p>The Forum recently completed a business case for the establishment of a regional healthy housing programme, that would have a central funding pool that local healthy housing initiatives could apply to deliver education around healthy homes, installation of insulation and heating, critical home repairs as required, and community DIY workshops for interventions such as draught stopping, curtains and improved energy use. The business case has been endorsed by the four major philanthropic funders (BayTrust, TECT, Rotorua Energy Trust and Eastern Bay Energy Trust.</p> <p><b>New Healthy Homes Standards</b> The forum made a submission to the healthy housing standards, which set minimum standards for heating, insulation, ventilation, moisture ingress and drainage, and draught-stopping. These standards have now been adopted.</p>	<p><u>Healthy Housing Business Case</u> The Forum and the four major philanthropic trusts are planning to establish a steering group, tasked with setting up the overall programme and fund.</p> <p>Projects such as A Healthy Whare could be a key beneficiary from the establishment of this programme and fund.</p> <p><u>Healthy Homes Standards</u> From July 2021, all private rental properties must comply with the standards within 90 days of any new tenancy. All private rentals must be compliant by 1 July 2024. Boarding houses must comply by 1 July 2021. MBIE's Tenancy Services Compliance and Investigation Team will ensure compliance with the standards, and is expanding its capacity to do this.</p>
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**SECURITY OF TENURE**

	<b>Actions</b>	<b>Background</b>	<b>Update</b>	<b>Next Steps &amp; Recommendations</b>
1.	Continue to monitor rental housing stock (as per requirements of the NPS).	The rental market and how it's operating is a driver of economic and social outcomes for the district. A high turnover of tenants and increasing rents	The latest Development Trends Report (released in December 2018) includes monitoring of the rental housing stock. The Development Trends Report shows that since mid 2014 there has been a steady increase in dwelling rents. The Report includes a ratio of sales prices to rent. The higher the ratio, the greater the gap between renting and buying, and the lower the average yield to an investor from renting out a dwellings. A high ratio may mean rental housing stock is constrained. The ratio has been on an upwards track since early 2015, rising from 23 to 28, but has flattened slightly over 2018.	Continue to monitor these indicators as part of Development Trends reporting.
2.	Advocate for and support central government initiatives that improve tenure security.	The average tenancy term in New Zealand is around 2 years. This leads to 'churn' in the rental housing market, with tenants moving frequently disrupting their work, education and family life. Improving tenure security reduces this 'churn' and the disruption it causes.	In August 2018 central government announced proposed changes to the Residential Tenancies Act 1986. Staff reviewed the proposed reforms and will have input as opportunities to comment are provided.	Staff will provide input into further work on the Residential Tenancies Act coming out of central government.

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**Western Bay of Plenty District Council**  
**Operations & Monitoring Committee**  
**Infrastructure Services Report April 2019**

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**Purpose**

To monitor and provide updates to the Operations & Monitoring Committee on current projects, contracts and works programmes.

**Recommendation**

- 1. THAT the Deputy Chief Executive's Report dated 26 March 2019 and titled Infrastructure Services Report April 2019 be received.*
- 2. THAT the Open Section of the Operations & Monitoring Committee Information Pack No. OP18 dated 26 March 2019 be received.*
- 3. THAT the report relates to an issue that is considered to be of low significance in terms of Council's Significance and Engagement Policy.*
- 4. THAT the petition received from Waihi Beach Ratepayers Association requesting Council to reconsider the decision and find a more suitable location for the KiwiCamp facility be received.*



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Gary Allis  
**Deputy Chief Executive Officer**



## 1. Utilities

### 1.1 Ongare Point Wastewater Scheme Preliminary Investigations

Council is currently building a Septic Tank Effluent Pump (STEP) wastewater system for the Ongare Point community.

The Treatment Plant construction is complete and is currently receiving waste from the connected tanks and is performing well. Only two properties are still required to be connected to the scheme. These properties have existing septic tanks that are being retrofitted to enable them to connect directly into the new scheme. The project is expected to be completed by the end of March.

### 1.2 Home Worm Composting Workshops

Dates for upcoming workshops for 2019 have been set. The information is available on Council's website at:

<http://www.westernbay.govt.nz/our-services/rubbish-recycling-waste/Pages/Worm-Composting.aspx>

All bookings are now made through [www.eventfinda.co.nz](http://www.eventfinda.co.nz)

### 1.3 Katikati Recycling Centre Opening Hours Trial

Please refer to separate report.

### 1.4 Te Puke Recycling Centre Extended Opening Hour Trial

Please refer to separate report

### 1.5 Katikati Alternative Options Assessment

The Katikati Wastewater Consent renewal was approved on 24 August 2018 and the Consent conditions requires Council to look at alternative options for the long-term discharge of the treated wastewater from the Katikati Wastewater Treatment Plant.

A working group has been formed that includes Tangata Whenua, Councillors and Community Board representatives. The group has been meeting on a regular basis to assess the various options available. Meetings held to date have discussed beneficial reuse of wastewater and were productive. Potential sites have been narrowed down for the beneficial reuse option and these sites are currently undergoing in-depth engineering assessment by technical consultants.

The in depth engineering assessment has been broken into three stages. Stage one looks at refining the sites selected for further investigations. It includes undertaking a planning assessment, technical assessment on appropriate disposal methods and developing success criteria to help determine the best method. Stage 1 is complete and was reported to the working group on 22 January, 2019. The engineering team is now looking at developing concept designs for the different disposal methods. These will be presented to the working group in May.

### 1.6 Highfields Pond

Council approved the raising of the water level trial during the February 2019 Operations and Monitoring Committee meeting. The trial will begin in October 2019.

### **1.7 Western Supply Zone Water Main Improvements**

Upgrade to water mains along Beach Road, Waihi Beach and replacement of water mains attached to Tuapiro Stream Bridge and Waitekohe Bridge on State Highway 2. Also a replacement of a short section of cross country water main between Lund Road and Hot Springs Road. Work has just started at Lund Road with the Waihi Beach section due mid March for completion in April 2019.

### **1.8 Two Mile Creek Update - Upstream Dillon Street Bridge**

The Bay of Plenty Regional Council Resource Consent was lodged in June 2017. Two parties did not sign in favour of applying for the Consent.

BOPRC requested further information be supplied by WBOPDC including modelling of the effects of the new channel on the 100-year flood and environmental and ecological issues with the new concrete channel. These assessments are now complete and final information has been provided to BOPRC. Feedback received from the BOPRC and draft Consent conditions have been finalised. BOPRC has provided the Resource Consent, so ready to go from this perspective.

Due to the creek being located in a floodable zone, a separate Resource Consent is required from WBOPDC. The Consent has been lodged and Council staff have been working with two property owners regarding submissions. One submission has been resolved, the other is still in negotiation. The Consent was expected to be granted in September 2018.

Direction being given to WBOPDC resource consent manager to proceed with arranging a Commissioner to hold a hearing to deal with the one party who is objecting to the proposal based on other development issues with their land.

The property owner who objected to the Consent has entered into discussions with the Strategic Property Manager to see if a solution can be found to his existing development and the adjoining Two Mile Creek Project. We are awaiting the outcome of this discussion.

Talks with the property owner have slowed. The decision has been taken to appoint a Hearing Commissioner with the date set for end of April 2019.

### **1.9 Comprehensive Stormwater Consents - Eastern Zone**

The Catchment Management Plan for the Eastern Catchment was lodged with the Bay of Plenty Regional Council prior to Christmas. The Eastern Zone includes the urban areas of Maketu, Te Puke, Pukehina and Paengaroa. The plan will form the basis for the Comprehensive Stormwater Consent Application. Consultation with the community was held on the Catchment Management Plan prior to it being lodged.

Positive feedback was received. This application is currently being processed by the Bay of Plenty Regional Council.

### **1.10 Te Puke Wastewater Treatment Plant Resource Consent Renewal**

The application to renew the Discharge Consent for the Te Puke Wastewater Treatment Plant has been lodged with the Bay of Plenty Regional Council. The application seeks a 35-year term. Submissions on the Consent application closed prior to Christmas. Sixty seven submissions were received on the application with the majority of the submissions in opposition to the application. Hearings have been set for the 15th – 18th April 2019.

The draft Consent conditions submitted with the Consent requires Council to look at alternative options for the long term discharge of the treated wastewater from the Te Puke Wastewater Treatment Plant. A working group has been formed, which includes Tangata Whenua, Councillors and Community Board representatives.

The group has been working to narrow down suitable sites for different disposal options including wetlands, pasture, forestry and reserve land. They have reduced the lists based on a set of criteria to a short list for each option. These will now be used for a high level engineering feasibility assessment. The engineering feasibility is underway.

#### **1.11 Te Puke Water Main Improvements**

Installation of new water mains in Moehau Street and Edgehill Place completed. New water mains to be installed in Station Road/Washer Place as well as Bayliss Road and part of No 3 Road. Works to start in April for completion end of May 2019.

#### **1.12 Comprehensive Stormwater Consents – Central Zone**

The Comprehensive Stormwater Consent (CSC) for the Central Zone (including Te Puna and Minden) was lodged with the Regional Council in 2017. A request for further information was received in February 2019. Staff are working with Councils consultants AECOM to address the further information request. The central CSC excludes Omokoroa as a CSC was obtained for Omokoroa in 2007.

#### **1.13 Kiwirail Bridge 91 ECMT Replacement - Access to the site**

Kiwirail have confirmed that they intend to replace the complete bridge that is located between the Council owned land of the Te Puke wastewater treatment plant and the existing Council Reserve.

The Physical works tender is going out end of January 2019 and at this stage no works will commence on site until the end of April 2019.

The cycleway section that passes under the bridge will need to be closed off for approximately 10 week period while works are undertaken. The contractor will have large machinery on site including a piling rig and large earth moving equipment/plant as such the area will be a high risk site with stringent Health and Safety procedures in place.

##### **Works include:**

- Piles and new concrete abutments being constructed
- Erosion protection works under the bridge area where the current stream passes under
- The existing embankment formations will be widened along the entire length
- New bridge deck
- New railway track installed.

Kiwirail have confirmed more detailed information will be provided closer to the construction phase once the contractor has been chosen. It is likely the works will be completed in two separate stages.

Kiwirail project manager has contacted Council to confirm and update progress of Bridge replacement. Contract documents have been prepared and registrations of interest to bid on the works are underway. Outcome of process may not be available until early April 2019.

Once the successful contractor has been announced a meeting will held with Council staff in attendance to confirm programme of works dates and closure of this section of the cycleway dates.

## 2. Reserves

### 2.1 Waihi Beach Ratepayers Association – Petition

A petition was received from Waihi Ratepayers Association requesting Council to reconsider the location their decision and find a more suitable place for the location of the KiwiCamp facility. **Attachment C**

### 2.2 KiwiCamp Facility

The KiwiCamp facility and installation at the community centre has been placed on hold. Alternative locations are being consulted through to mid-May. A decision on the location at Waihi Beach will be required by mid-year in order for installation by summer.

### 2.3 Kauri Point Jetty – Walkway Re-Decking

The walkway is being monitored for safety and any minor repairs undertaken in the interim, until complete walkway decking renewal can occur post landslip / road reinstatement access. This delay will extend into to 2019/2020.

### 2.4 Kauri Point Slip Repairs

The design has been broadened to improve storm water control which currently discharges in an uncontrolled manner in several locations near the slip zone. Work on the Assessment of Environmental Effects (AEE) i.e. part of a Resource Consent is underway.

Progress to date

- Concept design geotechnical and slope stability modelling completed
- Concept design presented to WBOPDC and agreed.
- Initial resource consent requirements scoping completed
- Commenced preparation of Assessment of Environmental Effects (AEE)

Planned works for February/March

- Additional investigation and design for stormwater management of the full Kauri Point

Reserve area

- Prepare consent issue drawing set
- Prepare Assessment of Environmental Effects
- Iwi consultation to commence
- Engage Archaeologist to prepare Archaeological Authority

Programme

- Currently on programme
- Proposed consent lodgement date is 27 March 2019 (we will look to lodge earlier than this anticipating additional time required due to iwi consultation)

Risks & Opportunities

- Risk of delays to consenting due to consultation requirements
- Risk of future landslips occurring elsewhere in the reserve and affecting the access road.

Mitigation of this risk by expanding the stormwater management scope to include the full reserve area.

#### Concept Design Notes

Design includes a rock buttress with the replacement road constructed above the buttress in the same alignment as the previous road.

Stormwater scope to include full Kauri point reserve area to try and minimise uncontrolled flows down the steep slopes. WBOPDC to arrange for their maintenance contractor to confirm the size, type and discharge location of all the stormwater assets in the reserve in particular the discharge point of the catchpits along Kyber Pass.

Discussed diversion of the road swale drain (upstream from the landslip) into the farmland to the west (which is on council road reserve). Council confirmed this approach.

Discussed modification of the existing stormwater inlet sump on the road opposite the landslip. Tonkin & Taylor to include in design.

Also discussed installation of a culvert across Chelmsford Street to divert water from the grassed reserve area into the council road reserve to the west.

Landslip debris shifted during the earthworks to be shaped at the base of the rock buttress and hydro-seeded initially. Council will then plant with Pohutukawa.

## 2.5 Haiku Park to Henry Road Cycleway/Walkway

The information/ interpretation sign-board has been installed.

## 2.6 Omokoroa to Tauranga Cycleway

Refer also to the report in the Information Pack and the story map on the website.

**Attachment B**

### User Statistics as follows:

Since trail opened the total count:

84,851 (63,329 pedestrians and 21,522 cyclists) with a daily average of 147 (110 & 37).  
 Approximately 25 % of users are cyclists.

**Sections 1 & 2** (through to Lynley Park) have been completed.

### Section 3:

Lynley Park to railway - The archaeological authority application remains delayed while further information required by Heritage New Zealand is provided. This will be resubmitted soon.

### Section 4:

Tamihana Park to Kiwirail Bridge – No change since last report. The final surfacing along this section is completed although the track remains closed until railway embankment repair, Kiwirail approvals, & lease agreement in place. PowerCo and Ultra Fast Fibre have finished installing buried ducting along this section to provide services for future Omokoroa growth. The step section is to be sealed.

The official opening of this section will be coordinated with the Plummers Point section to Huharua Park so that a route destination is enabled. Completion expected April/May 2019.

**Section 5:**

Bridge across Mangawhai inlet – completed but the track will remain closed until the cycleway connection with Huharua Park is in place.

**Section 6:**

Plummers Point trail from bridge to Huharua Park is now well advanced and completion will be coordinated with section 4.

**NOTE:** The commuter route section proposed to connect between Bridge1 and Plummers Point Road is being constructed in liaison with the private property owners. Completion of this will also be timed with sections 4 & 6 to provide a trail loop & commuter connection.

**2.7 Omokoroa Sportsground Playground**

The playground is being well used. An official opening is planned for the first school term of 2019 (date to be confirmed).

Final additions involve shrub garden planting and additional trees, which will be planted during autumn.

**2.8 Omokoroa Ferry to Opureora/Matakana Island – Facilities Upgrade**

The bulletin board (confirmed by the Island community) is yet to be purchased and installed

**2.9 Omokoroa Slips**

A proposal to repair a slip from No 69 Omokoroa Road onto Council walkway has been approved in principle. An agreement has been completed between Council and the owner which will enable the owner to use Council's reserve in order to effect the repair. The works proposal has also been submitted for Consents.

Staff from EQC have indicated an interest in the works completed at Ruamoana Walkway. It was suggested that EQC may proactively trial the chimney drain methodology elsewhere in NZ. The Waikato University research student is making steady progress with his stability project with ongoing analysis and research.

Council has required 37 McDonnell Street for a nominal sum. The house will be removed, the top of the bank shaped, fenced and planted.

**2.10 Opureora Marae Public Toilet**

Staff met with the Marae Committee on 25 January 2019 to discuss contract issues and the planned toilet building upgrade through the maintenance contractor.

An asbestos inspection has been undertaken in preparation for these repairs.

**2.11 Waihi Beach Cycleways**

Planning processes continue to progress a shared use loop trail through the water catchment reserve. Construction timing is dependent on Archaeological Authority application processing timeframe and funding.

A new clip-on cycleway bridge at 3-Mile Creek (Seaforth Road) is underway.

It is noted that the recently completed section through Island View Reserve has not been located as originally intended, instead, it connects to a much narrower gravel path alongside the carpark - which can conflict with overhanging parked cars & motorhomes. To improve the user experience, it is proposed that the carpark fence is removed, which will then allow for the widening and resurfacing of the gravel path.

The detailed design is underway for the Emerton Road shared path, this will be located on the harbour side of Emerton Road.

Land owner discussion is continuing for the Waihi to Waihi Beach Cycle Trail.

### 2.12 Trig Walkway Use Statistics

Total Traffic for the period analysed: 1 Aug 2017 to 19 Mar 2019	111,334
Total Daily average:	187
Weekday Average:	168
Weekend Average:	233
Max average value January:	359
Min average value August:	89

### 2.13 Waihi Beach Dam – New Toilet & Site Improvement Concept Plan

Contractors have been arranged to install water & waste-water services for the connection of the Exeloo toilet that has been relocated from Wilson Road, adjacent to the old depot building.

This toilet has been returned to the Exeloo factory for a "make-over" prior to being installed. Other features will be completed during this development including footpath connections, converting the buildings "lean to" into a recreation space trail head (undercover table/ seating, signage, potential BBQ facilities, bike racks).

It is also proposed that community artists may wish to paint artworks onto the building as a community led project. Overflow car-parking space is also being reviewed to see if a gravel car-park could be constructed on the other side of the driveway prior to the dam itself. Completion for all of these is expected late April to mid May.

### 2.14 Dotterel Point Reserve Capital Development (Pukehina)

There has been little change since the previous report. The monitoring of the upgraded septic tank system will remain ongoing and likely to remain this way until the Surf Club begin construction of their new facilities, the timing of which is subject to their funding being secured.

### 2.15 Rotoiti to Paengaroa Cycle Trail

Construction of this trail is nearing completion and an official opening will be planned for late April or May 2019 - date to be advised. A multi-counter has been installed to provide user statistics.

Note: This project is fully funded by NZTA.

### **3. Strategic Property**

#### **3.1 Katikati Library**

Staff have approved the design of the mezzanine barrier and it is being fabricated off-site. Once installed, the final Code of Compliance Certificate will be issued.

#### **3.2 Te Puna Hall**

The site blessing has been held. Staff are currently seeking a Resource Consent for construction of the Hall on McIntyre land.

#### **3.3 Omokoroa Library & Service Station**

Regular meetings are being held with all parties to progress the design and future construction. The Building Consent has been programmed to be lodged during March 2019. A pre-lodgement meeting will be held with building control officials.

#### **3.4 Kaimai Views - Sales Update**

Sale for stages two and three progressing well. The developer is in the process of purchasing these two stages and will see income for the sale of the land and receipt of financial contributions.

### **4. Engineering & Special Projects**

#### **4.1 Te Puke Highways - Pah Road Slip Reinstatement**

While the Resource Consent has been granted by Bay of Plenty Regional Council for the reinstatement of the Pah Road slip, New Zealand Transport Agency has not confirmed the timing for commencement of this project. It is likely that this project will be delayed and now constructed in the next four years when funding is made available by NZTA.

#### **4.2 Omokoroa Industrial Road – Stormwater Detention Pond – Investigation, Design & Construction**

The design is well advanced. The landowner discussions are occurring. Once the land issues are sorted, tenders will be invited for the construction work and for completion in 2019/20 financial year.

#### **4.3 Omokoroa Road Urbanisation – Western Avenue to Tralee Street**

The section of Omokoroa Road from Western Avenue to Tralee Street including the Tralee Street intersection is currently being designed for construction in the 2018/19 and 2019/20 financial years subject to NZTA funding approval with construction unlikely this construction season.

The current Omokoroa Road will be widened from 7.0m to 11.5m with provision for one lane of traffic in each direction with right turning facilities at several intersections. The road will include a 1.5m footpath on the western side and a 2.5m shared pedestrian and cycle path on the eastern side. There will be several bus stops placed at strategic locations on both sides of the Omokoroa road. Two bus stops opposite the Fresh Choice supermarket on Tralee Street will also be installed.



Lighting will be included to illuminate both the on-road traffic and off-road pedestrian and cycle traffic.

Power will remain as overhead and the space between kerb and channel and road boundary will be grassed and planted similar to the section south of Western Avenue.

The Tralee Street intersection will include a roundabout to facilitate traffic movements and improve safety of the intersection.

Please note, that timing of the work is subject to approval of NZTA subsidy and if this is not approved, the scope of the project will need to be reviewed.

#### **4.4 Omokoroa Stormwater PO2 Construction and Earthworks**

The construction of the Omokoroa stormwater is being undertaken by Map Projects for the \$3.04M. Despite heavy rainfall during November/December 2018, the contractor is making good progress in carrying out the earthworks for the HAF area and forming the dam. It is expected that a substantial proportion of the contract work will be completed by the end of this financial year and the remaining work associated with the SHA area will be completed in the following financial year.

The stormwater retention pond when completed, will accommodate the stormwater runoff from the residential development of the special housing area, Goldstone block, Neil Group block and areas beyond the currently zoned urban land. The planting in the gully and access tracks will be undertaken by a separate contractor in the 2018/19 financial year.

#### **4.5 Western Avenue to Kayelene Place Cycleway**

The cycle and pedestrian link between Western Avenue and Kayelene Place is currently being designed and for construction in the 2018/19 and 2019/20 financial years. The project includes 2.5m concrete/metalled pedestrian/ cycle path on the future Hamurana Road alignment. The project will also include piping the existing two gullies which flows across the Hamurana Road Reserve with 1600mm and 1200mm diameter pipes and filling onto to enable the construction of the pedestrian and cycleway. It is intended to use the existing fill material placed on the Hamurana Road Reserve at the Kayelene Street end of the project to reduce the cost of this project.

#### **4.6 Review of Te Puke Main Street**

The Te Puke Main Street upgrade was reviewed after 12 months of operation and a report dealing with parking, vegetation, the plaza and traffic was presented to the Operations and Monitoring Committee Meeting on 28 February 2019. The Committee received the recommendations of the report however, agreed to refer the report to the Te Puke Community Board for their comment and for further consideration by the Committee a future meeting. In the meantime, the Committee agreed to replace the vegetation at several intersections to improve visibility and safety for motorists and pedestrians. The staff are currently reviewing the roadside plantings with the Councillor and Te Puke Community Board representatives with the view to replace the plantings in autumn.

## 5. Emergency Management

### 5.1 Community Response Plan

A community response plan covers how the community plans to self-activate and respond in the initial stages of a large scale emergency. If robust processes and systems are working well then the civil defence response will support what is already underway, not override the community response.

The plan is developed by community champions and/ or representatives of stakeholder organisations such as local schools, businesses, Neighbourhood Support, Lions, Resident Associations and so on. Ownership of the plan remains with the community, represented by the stakeholder group.

### 5.2 Maketu

The Community response team is working on updating their community response plan and they are also looking for some new people to join their team as some people left the team.

We are waiting to hear back from the team about their progress.

### 5.3 Te Puke

The Community Response team has an update to date community response plan in place. Team is keen to do a desktop exercise soon, planning stage at the moment. Also working with the team to increase the public awareness of the team and the promotion of personal preparedness for an emergency.

### 5.4 Waihi Beach

The Community Response team has an up to date Community Response Plan in place. Currently we are assisting the team to attract new team members, as some people have left the area.

### 5.5 Omokoroa

The Community Response Team is working on updating their community response plan and they are also looking for some new people to join their team, as some people left the team. We are waiting to hear back from the team about their progress.

### 5.6 Katikati

The Community Response Team has an up to date community response plan in place. Currently we are assisting the team to attract new team members, as some people have left the area, in addition to working with the Community Response Team to increase the public awareness of the team and the promotion of personal preparedness for an emergency.

## 6. Roading

### 6.1 One Network Maintenance Contract (ONMC)

Refer to OP18 Information Pack.

**Attachment A**

## 6.2 Katikati Bypass Update

There has been no further change since the previous update and is still pending further announcements from the New Zealand Transport Agency, staff have not yet been advised of a date.

## 6.3 Omokoroa Road Construction - State Highway to Railway

The Omokoroa Road upgrade has been an exceptionally complex project due to weather delays, shortage of construction crews, roundabout and intersection builds, adjoining developer requests and the construction of multiple underground services with different utility authority ownership.

Most of the new road pavement has now been completed. The \$12.9million project, with the physical works beginning early in 2017, also hit setbacks with the discovery of multiple archaeological sites and the removal of redundant asbestos water main pipes requiring specialist handling and disposal.

Underground services installed during the construction have included new water mains, a new gravity sewer main, a new storm water main, new road culverts, communications and power cables, Ultrafast broadband fibre optic cabling and the relocation of sections of the Omokoroa sewer transfer main. The sewer transfer main air value, which was situated under the road shoulder, has now been relocated during an overnight shutdown.

The remaining construction works have included the street light installations, sections of the cycleway, berm top soiling, second coat surfacing and final pavement markings. A section of the cycleway will not be surfaced with concrete in the area adjacent to the planned industrial road intersection because its final level will be determined by the new kerbing levels when they are designed and constructed. The roads chip seal surfaces have got their second coat seals and asphalt surfacing has been applied to the Omokoroa Carriers entranceway intersection to accommodate the heavy traffic turning requirements. The timing of the sewer main duplication into the transfer station will depend on suitably qualified drain layers being available to undertake the work in the New Year. The vegetation maintenance requirements and levels of service for this area are being developed. In the short term this will include berm mowing and after the developers maintenance period ends the planted embankments requirements.

## 6.4 Maketu Road Proposed Cycle Trail

All survey work is complete. Detailed design and Archaeological Authority are underway by WestLink. Completed design drawings should be complete by mid to late April.

## 6.5 Wilson Road Footpath Bridge

As a result of additional investigations the flat deck bridge option was proposed to the Community Board at the recent Community Board Workshop. The Board indicated that the flat deck bridge option is an ideal choice.

It must be noted that the flat rack bridge may not be consented by Council, hence will be considered a temporary structure.

## 6.6 Wilson Road Pedestrian Diversion and Kerb Extension

This project has been assigned to WestLink. Construction is expected to commence mid April 2019.

### **6.7 Kotahi Tree Rationalisation**

The Katikati Community Board have requested that the Kotahi Lane berm side parking be reassessed and improved to make parking more accessible in this area. This improvement includes removing nine of the seventeen cherry trees within the northern side berm. The current tree arrangement on Kotahi Lane is considered cluttered. Nine of the seventeen trees have never "taken" and add little aesthetic value to the area.

It is expected the tree clearance and maintenance work will create additional car park spaces in addition to reducing the amount of detritus from falling blossom, leaf, branches and bird droppings onto the parked cars. The remaining eight cherry trees are considered to provide sufficient beautification to the retail and light industrial area of Kotahi Lane.

These work are currently in consultation and is likely to be undertaken in mid April, once consultation has been completed.

### **6.8 Henry Road**

The Henry Road footpath option 3 (2.5 metre wide footpath with kerb and channel) has been approved by the Katikati Community Board. The Contractor has been requested to complete the works as agreed.

### **6.9 Beach Road**

The Katikati Community Board have agreed that the Beach Road footpath will not be considered for design or construction until further notice.

### **6.10 Middlebrook Drive Bus Shelter**

The Katikati Community Board have requested a bus shelter be constructed on Middlebrook Drive to encourage people to use the park and ride bus service. This has now been resolved and discussions have commenced with the manufacturer.

### **6.11 Waihi Beach CCTV Request**

The CCTV Policy is currently being reviewed internally by Council staff.

### **6.12 Seaforth Road Pedestrian Islands**

WestLink are finalising the designs so as to start construction this year. Consultation may be required with the dairy operator, in particular if parking spaces are lost further to the implementing of the pedestrian refuge at the "Waihi Beach" - Seaforth Road site.

### **6.13 Earl Drive Loading and Parking Restructure**

Further to discussions with the Katikati Community Board and Earl Drive businesses, a review of the parking and loading arrangement has been requested. The three concept design options have been considered by the community board with Option 1 - Truck waiting bay on the northern side of Earl Drive being agreed and approved by the Community Board. The detailed design will be progressed and presented to the Community Board in the coming months.